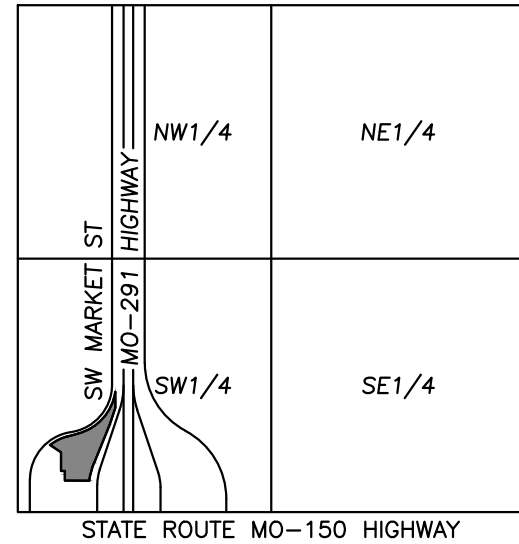


LEGEND	
— PL —	PROPERTY LINE
- - - LL - -	LOT LINE
- - - R/W - -	RIGHT-OF-WAY
=====	2' CURB & GUTTER
=====	6" CURB
— B/L —	BUILDING SETBACK LINE
— P/S —	PARKING SETBACK LINE
[Pattern]	STANDARD DUTY ASPHALT PAVEMENT
[Pattern]	PROPOSED BUILDING
[Pattern]	CONCRETE PAVEMENT
[Pattern]	CONCRETE SIDEWALK



VICINITY MAP
SEC. 29-47-31



BUILDING & LOT DATA	
Zoning	CP-2
Lot 2	
Site Area	2.58 Ac.
Home Improvement	
No. of Stories	1 Story
Building S.F.	15,200 S.F.
Building Footprint	15,200 S.F.
FAR / Building Coverage	0.1352
Impervious Area	1.78 Ac. (69%)
Open Space	0.80 Ac. (31%)

PARKING SUMMARY	
Lot 2	
Parking Required:	
Home Improvement (4 / 1,000 S.F.)	
Total Required Parking	61 Spaces
Parking Provided:	
Standard Parking Provided	86 Spaces
Accessible Parking Spaces Provided	4 Spaces
Total Provided Parking	90 Spaces
Parking Lot, Aisles, and Drives Area	47,000 SF
Parking Lot Landscape Islands, Strips, Planting Areas	8,000 SF (17%)



UTILITY NOTES:
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LEGAL DESCRIPTION:
LOT 2, MARKET STREET CENTER, A SUBDIVISION OF LAND IN THE CITY OF LEE'S SUMMIT, JACKSON COUNTY, MISSOURI.
AREA = ± 2.5822 ACRES / ± 112,481.21 SQ.FT.

- SITE PLAN NOTES:**
- All construction materials and procedures on this project shall conform to the latest revision of the following governing requirements, incorporated herein by reference:
A) City ordinances & O.S.H.A. Regulations.
B) The City of Lee's Summit Technical Specifications and Municipal Code.
 - The contractor shall have one (1) signed copy of the plans (approved by the City) and one (1) copy of the appropriate Design and Construction Standards and Specifications at the job site at all times.
 - The contractor will be responsible for securing all permits, bonds and insurance required by the contract documents, City of Lee's Summit, Missouri, and all other governing agencies (including local, county, state and federal authorities) having jurisdiction over the work proposed by these construction documents. The cost for all permits, bonds and insurance shall be the contractors responsibility and shall be included in the bid for the work.
 - The contractor is responsible for coordination of his and his sub-contractor's work. The contractor shall assume all responsibility for protecting and maintaining his work during the construction period and between the various trades/sub-contractors constructing the work.
 - The demolition and removal(or relocation) of existing pavement, curbs, structures, utilities, and all other features necessary to construct the proposed improvements, shall be performed by the contractor. All waste material removed during construction shall be disposed off the project site. The contractor shall be responsible for all permits for hauling and disposing of waste material. The disposal of waste material shall be in accordance with all local, state and federal regulations.
 - Contractor shall be responsible for all relocations, including but not limited to, all utilities, storm drainage, sanitary sewer services, signs, traffic signals & poles, etc. as required. All work shall be in accordance with governing authorities specifications and shall be approved by such. All cost shall be included in base bid.
 - All existing utilities indicated on the drawings are according to the best information available to the Engineer; however, all utilities actually existing may not be shown. The contractor shall be responsible for contacting all utility companies for an exact field location of each utility prior to any construction. All underground utilities shall be protected at the contractor's expense. All utilities, shown and unknown, damaged through the negligence of the contractor shall be repaired or replaced by the contractor at his expense.
 - The contractor will be responsible for all damage to existing utilities, pavement, fences, structures and other features not designated for removal. The contractor shall repair all damages at his expense.
 - The contractor shall verify the flow lines of all existing storm or sanitary sewer connections and utility crossings prior to the start of construction. Notify the engineer of any discrepancies.
 - SAFETY NOTICE TO CONTRACTOR:** In accordance with generally accepted construction practices, the contractor shall be solely and completely responsible for conditions of the job site, including safety of all persons and property during performance of the work. This requirement will apply continuously and not be limited to normal working hours. Any construction observation by the engineer of the contractor's performance is not intended to include review of the adequacy of the contractor's safety measures, in, on or near the construction site.
 - Refer to the building plans for site lighting electrical requirements, including conduits, pole bases, pull boxes, etc.

SITE DIMENSION NOTES:
1. BUILDING TIES SHOWN ARE TO THE OUTSIDE FACE OF PROPOSED WALLS. THE SUBCONTRACTOR SHALL REFER TO THE ARCHITECTURAL PLANS FOR SPECIFIC DIMENSIONS AND LAYOUT INFORMATION FOR THE BUILDINGS.
2. ALL DIMENSIONS SHOWN FOR THE PARKING LOT AND CURBS ARE MEASURED FORM BACK OF CURB TO BACK OF CURB.

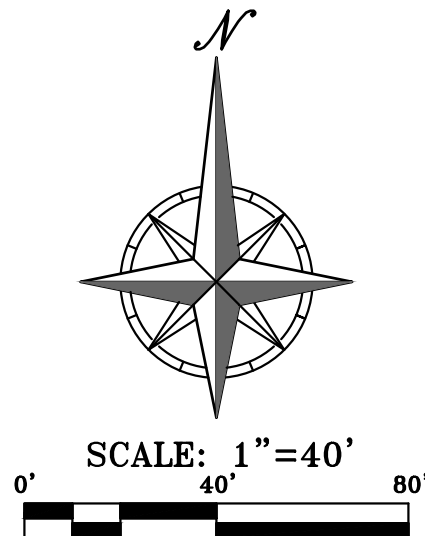
PAVEMENT MARKING AND SIGNAGE NOTES:
1. PARKING STALL MARKING STRIPES SHALL BE FOUR INCH (4") WIDE WHITE STRIPES. DIRECTIONAL ARROW AND HANDICAP STALL MARKINGS SHALL BE FURNISHED AT LOCATIONS SHOWN ON PLANS.
2. HANDICAP PAVEMENT MARKINGS AND SIGNS SHALL CONFORM TO ALL FEDERAL (AMERICANS WITH DISABILITIES ACT) AND STATE LAWS AND REGULATIONS.
3. TRAFFIC CONTROL DEVICES AND PAVEMENT MARKINGS SHALL CONFORM TO THE REQUIREMENTS OF THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES".
4. STOP SIGNS SHALL BE PROVIDED AT ALL LOCATIONS AS SHOWN ON PLANS AND SHALL CONFORM TO THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES". SIGNS SHALL BE 18" X 12", 18 GAUGE STEEL AND SHALL BE ENGINEER GRADE REFLECTIVE.
5. TRAFFIC CONTROL AND PAVEMENT MARKINGS SHALL BE PAINTED WITH A WHITE SHERWIN WILLIAMS S-W TRAFFIC MARKING SERIES B-29Y2 OR APPROVED EQUAL. THE PAVEMENT MARKING SHALL BE APPLIED IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS. APPLY ON A CLEAN, DRY SURFACE AND AT A SURFACE TEMPERATURE OF NOT LESS THAN 70°F AND THE AMBIENT AIR TEMPERATURE SHALL NOT BE LESS THAN 60°F AND RISING. TWO COATS SHALL BE APPLIED.

ZONING:
THIS PROPERTY IS ZONED CP-2, DEFINED AS PLANNED COMMUNITY COMMERCIAL DISTRICT.

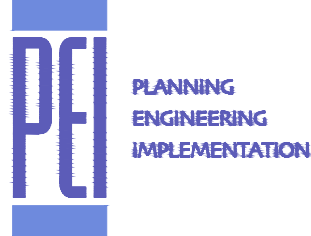
OIL-GAS WELLS:
ACCORDING TO THE MISSOURI DEPARTMENT OF NATURAL RESOURCES STATE OIL & GAS COUNCIL WELLS, LOCATED AT www.dnr.mo.gov/geology/geosrv/oilandgas.htm, THERE ARE NO OIL OR GAS WELLS ON THE PROPERTY SHOWN HEREON.

PRE-CONSTRUCTION MEETING NOTE:
THE CONTRACTOR SHALL CONTACT THE CITY'S DEVELOPMENT SERVICES ENGINEERING INSPECTION TO SCHEDULE A PRE-CONSTRUCTION MEETING WITH A FIELD ENGINEERING INSPECTOR PRIOR TO ANY LAND DISTURBANCE WORK AT (816) 969-1200.

FIRE ACCESS ROAD NOTE:
ALL FIRE ACCESS LANES SHALL BE HEAVY DUTY ASPHALT CAPABLE OF SUPPORTING 75,000-POUNDS.



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a new store for
Westlake ACE Hardware
3511 SW Market Street
Lee's Summit, Missouri 64082

date
drawn by
checked by
revisions
1. 09-13-22
REVISED PER PLANNING COMMENTS

SITE PLAN
sheet number
C1
drawing type
PDP
project number
22185

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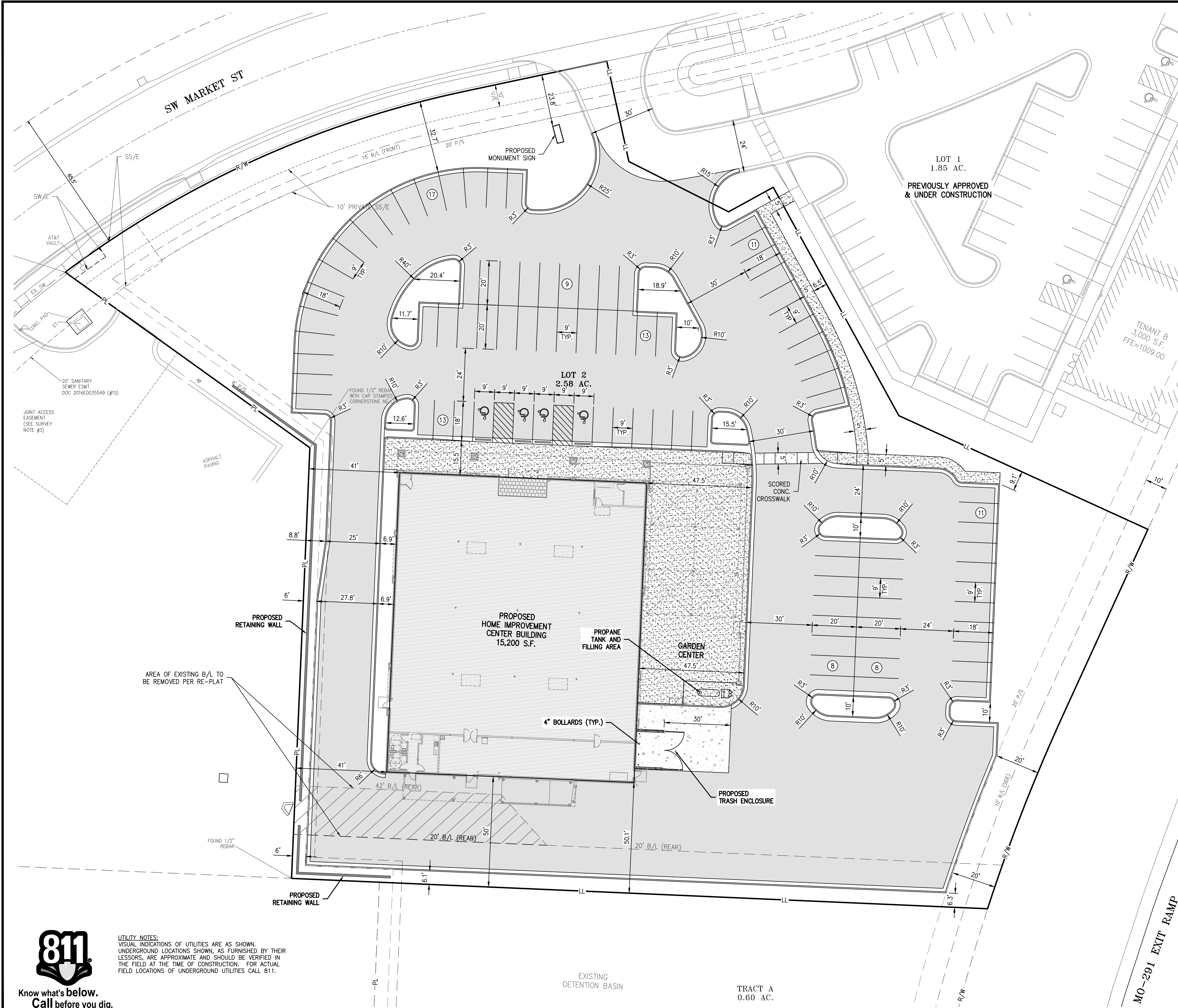
ENLARGED
SITE
PLAN

sheet number

C1.1

drawing type
PDP

project number
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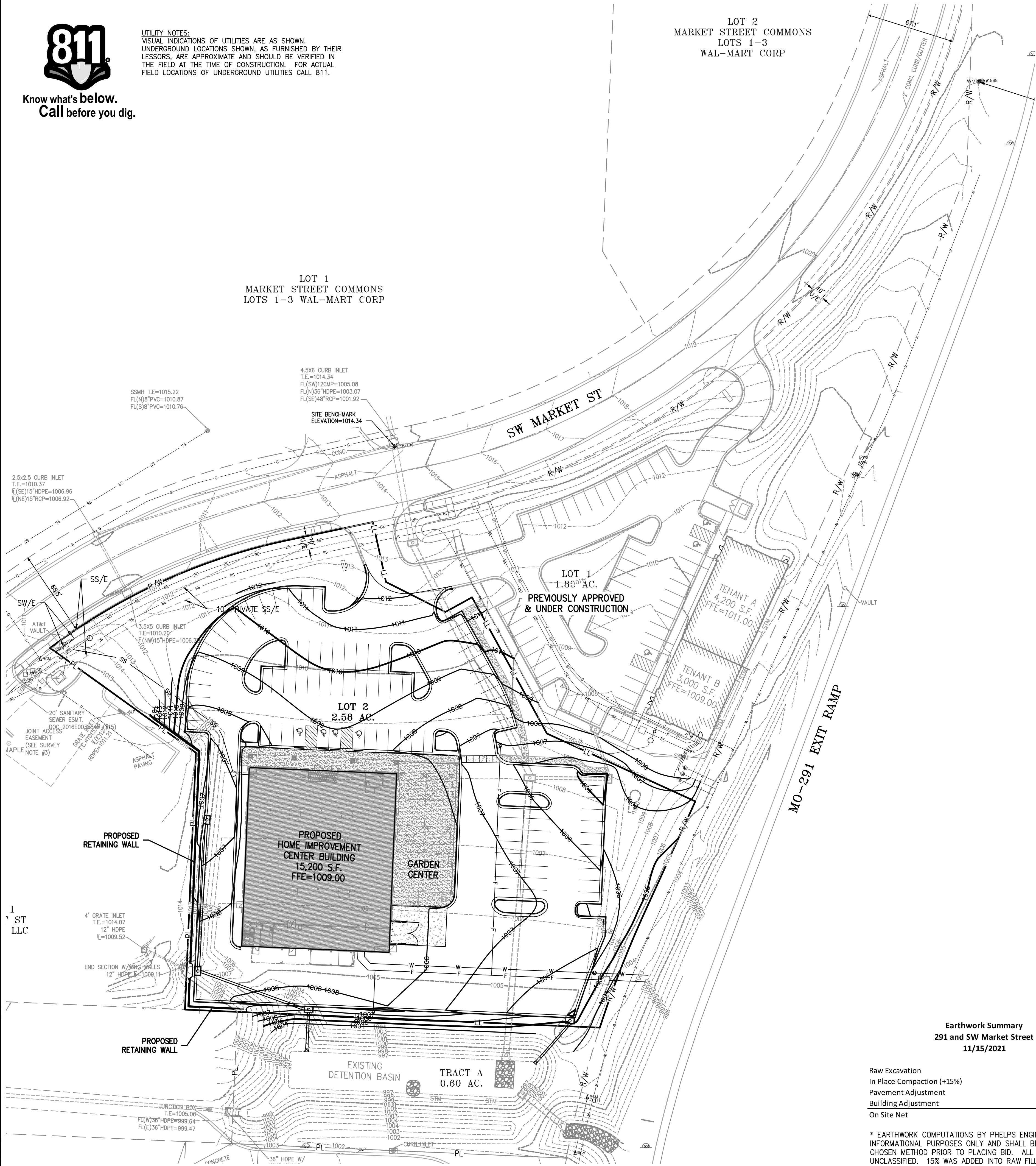


Know what's below.
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LOT 2
MARKET STREET COMMONS
LOTS 1-3
WAL-MART CORP

LOT 1
MARKET STREET COMMONS
LOTS 1-3 WAL-MART CORP



SITE GRADING NOTES:

1. CONTOURS AND ELEVATIONS: Existing and proposed contours are shown on plans at one foot (1') contour intervals, unless otherwise noted, proposed contours and elevations shown represent approximate finish grade. Contractor shall hold down subgrades to allow for pavement and sub-base thicknesses.
2. If the contractor does not accept existing topography as shown on the plans, without exception, he shall have made at his expense, a topographic survey by a registered land surveyor and submit it to the owner for review.
3. CLEARING AND GRUBBING: Prior to beginning preparation of subgrade, all areas under pavements or building shall be stripped of all topsoil, vegetation, large rock fragments (greater than 6 inches in any dimension) and any other deleterious material. The actual stripping depth should be based on visual examination during construction and the results of proof-rolling operations. The root systems of all trees (not designated to remain) shall be removed in their entirety. Stripping materials shall not be incorporated into structural fills.
4. TOPSOIL STRIPPING: Prior to the start of site grading, the contractor shall strip all topsoil from areas to be graded, and stockpiled at a location on or adjacent to the site as directed by the owner. At completion of grading operations and related construction, the contractor will be responsible for redistribution of topsoil over all areas disturbed by the construction activities. Topsoil shall be placed to a minimum depth of six inches (6") and in accordance with specifications for landscaping. At that time, and prior to the installation of landscaping or irrigation, all topsoil graded areas shall be visually inspected and accepted by the owner and I.T.L.
5. Contractor shall adjust and/or cut existing pavement as necessary to assure a smooth fit and continuous grade. Contractor shall assure positive drainage away from buildings for all natural and paved areas.
6. SUBGRADE PREPARATION: Prior to placement of new fill material, the existing subgrade shall be proofrolled and approved under the direction of the Geotechnical Engineer or his representative.
7. PROOFROLLING: Subsequent to completion of stripping and over-excavation, all building and pavement areas to receive engineered fill should be systematically proof-rolled using a tandem axle dump truck loaded to approximately 20,000 pounds per axle. Also, any finished subgrade areas to receive paving shall be proof-rolled within 48 hours of paving. Unsuitable soils that are detected and that can not be recompacted should be over-excavated and replaced with controlled structural fill.
8. EARTHWORK:
 - A) GEOTECHNICAL: All earthwork shall conform to the recommendations of the Geotechnical report. Said report and its recommendations are herein incorporated into the project requirements by reference. Prior to beginning construction, the contractor shall obtain a copy of and become familiar with the geotechnical report. Unless specifically noted on the plans, the recommendations in the geotechnical report are hereby incorporated into the project requirements and specifications.
 - B) SURFACE WATER: Surface water shall be intercepted and diverted during the placement of fill.
 - C) FILLS: All fills shall be considered controlled or structural fill and shall be free of vegetation, organic matter, topsoil and debris. In areas where the thickness of the engineered fill is greater than five feet, building and pavement construction should not commence until so authorized by the on-site geotechnical engineer to allow for consolidation.
 - D) BUILDING SUBGRADE: As specified in the Geotechnical Engineering Report, the upper section of building subgrade shall consist of Low Volume Change (LVC) material defined as approved, compacted granular fill or low to moderate plasticity cohesive soil materials stabilized with Class C Flyash. Granular fill shall consist of compacted granular materials with a maximum particle size of two (2) inches or less, such as limestone screenings. Refer to geotechnical report for complete requirements.
 - E) EXISTING SLOPES: Where fill material is to be placed on existing slopes greater than 5:1 (horizontal to vertical), existing slope shall be benched providing a minimum vertical face of twelve inches (12"). The benches should be cut wide enough to accommodate the compaction equipment. Fill material shall be placed and compacted in horizontal lifts not exceeding nine inches (9") (loose lift measurement), unless otherwise approved by the Geotechnical Engineer.
 - F) COMPACTION REQUIREMENTS: The upper 9 inches of pavement subgrade areas shall be compacted to a minimum density of ninety five percent (95%) of the material's maximum dry density as determined by ASTM D698 (standard proctor compaction). The moisture content at the time of placement and compaction shall within a range of 0% below to 4% above optimum moisture content as defined by the standard proctor compaction procedure. The moisture contents shall be maintained within this range until completion of the work. Where compaction of earth fill by a large roller is impractical or undesirable, the earth fill shall be hand compacted with small vibrating rollers or mechanical tampers.
9. All cut or fill slopes shall be 3:1 or flatter. All asphalt parking areas shall be a minimum of 1% slope but not more than 5% slope unless otherwise noted. All pavements within ADA parking areas shall not exceed 2% total slope. All grades around building shall be held down 6" from finish floor and slope away another 6" in 10 feet. Contractor shall notify engineer prior to find subgrade construction of any areas not within this slope requirement.
10. TESTING AND INSPECTION: Owner's Independent Testing Laboratory (ITL) shall make tests of earthwork during construction and observe the placement of fills and other work performed on this project to verify that work has been completed in accordance with Geotechnical Engineering Report, Project Specifications and within industry standards. The ITL will be selected by the owner and the cost of testing will be the owner's responsibility.
11. CLASSIFICATION: All excavation shall be considered unclassified. No separate or additional payments shall be made for rock excavation.
12. PERMANENT RESTORATION: All areas disturbed by earthwork operations shall be sodded, unless shown otherwise by the landscaping plan or erosion control plan.
13. UTILITIES: The contractor is specifically cautioned that the location and/or elevation of existing utilities as shown on these plans is based on records of the various utility companies, and where possible, measurements taken in the field. The information is not to be relied on as being exact or complete. The contractor must call the appropriate utility companies at least 48 hours before any excavation to request exact field location of utilities. It shall be the responsibility of the contractor to relocate all existing utilities which conflict with the proposed improvements shown on the plans.
14. LAND DISTURBANCE: The contractor shall adhere to all terms & conditions as outlined in the EPA or applicable state N.P.D.E.S. permit for storm water discharge associated with construction activities. Refer to project S.W.P.P.P. requirements.

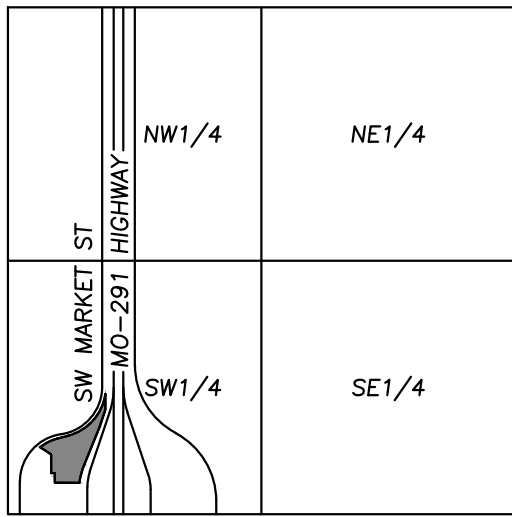
FLOOD NOTE:

THIS PROPERTY LIES WITHIN ZONE X, DEFINED AS AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN, AS SHOWN ON THE FLOOD INSURANCE RATE MAP PREPARED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY FOR THE CITY OF LEE'S SUMMIT, COMMUNITY NO. 290174, JACKSON COUNTY, MISSOURI, MAP NO. 29095C04366, AND DATED JANUARY 20, 2017.

BENCHMARK:

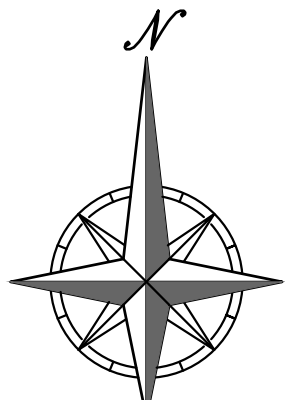
VERTICAL DATUM = NAVD88 BASED ON GPS OBSERVATION USING MDOOT VRS

1. FOUND "C" CUT IN CONCRETE SIDEWALK AT SOUTHWEST CORNER OF ADJACENT PROPERTY. ELEVATION = 987.14
2. SET "L" CUT IN SOUTHWEST CORNER OF BACK OF CURB IN ADJACENT PARKING LOT TO THE NORTH AT NORTHWEST CORNER OF SURVEYED PROPERTY. ELEVATION = 990.19



LEGEND

- | | |
|----------------------|----------------------------|
| — PL — | PROPERTY LINE |
| — LL — | LOT LINE |
| — R/W — | RIGHT-OF-WAY |
| — 2' CURB & GUTTER — | |
| — 920 — | EXISTING CONTOURS |
| — 920 — | PROPOSED CONTOURS |
| XXX.XX | PROPOSED SPOT ELEVATION |
| TW | TOP OF WALL |
| LG | LIP OF GUTTER |
| TC | TOP OF CURB |
| SW | SIDEWALK |
| ME | MATCH EXISTING |
| HP | HIGH POINT |
| LP | LOW POINT |
| P | TOP OF PAVEMENT |
| TE | TOP OF STRUCTURE |
| GR | GROUND ELEVATION |
| BS | BOTTOM OF STEPS |
| TS | TOP OF STEPS |
| BW | BOTTOM OF WALL |
| TW | TOP OF WALL |
| — — — | EXISTING STORM SEWER |
| — — — | PROPOSED STORM PIPE |
| — — — | PROPOSED WET CURB & GUTTER |
| — — — | PROPOSED DRY CURB & GUTTER |
| — — — | PROPOSED RETAINING WALL |

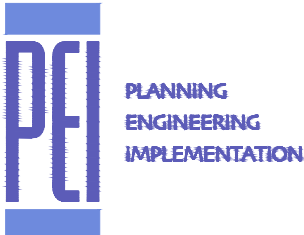


Earthwork Summary
291 and SW Market Street
11/15/2021

Raw Excavation	4,799 Cu. Yds.
In Place Compaction (+15%)	-16,085 Cu. Yds.
Pavement Adjustment	2,379 Cu. Yds. (assume 10" of additional excavation)
Building Adjustment	1,807 Cu. Yds. (assume 24" of additional excavation)
On Site Net	-7,099 Cu. Yds.

* EARTHWORK COMPUTATIONS BY PHELPS ENGINEERING, INC. ARE PROVIDED FOR INFORMATIONAL PURPOSES ONLY AND SHALL BE VERIFIED BY CONTRACTORS BY THEIR CHOSEN METHOD PRIOR TO PLACING BID. ALL EARTHWORK SHALL BE CONSIDERED UNCLASSIFIED. 15% WAS ADDED INTO RAW FILL QUANTITY TO ACCOUNT FOR SHRINKAGE.

4301 Indian Creek Parkway
Overland Park, KS 66207
phone: 813.451.9390
fax: 813.451.9391
www.davidsonae.com



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date

drawn by

checked by

revisions

1. 09-13-22
REVISED PER PLANNING
COMMENTS

GRADING
PLAN

sheet number

C2

drawing type
PDP

project number
22185

\\PHILIPS-SERVER\Projects\Project\220895\Draw\Preliminary\GROUNDING.dwg Layout:2 Sep 13, 2022 2:43pm Donita Finn



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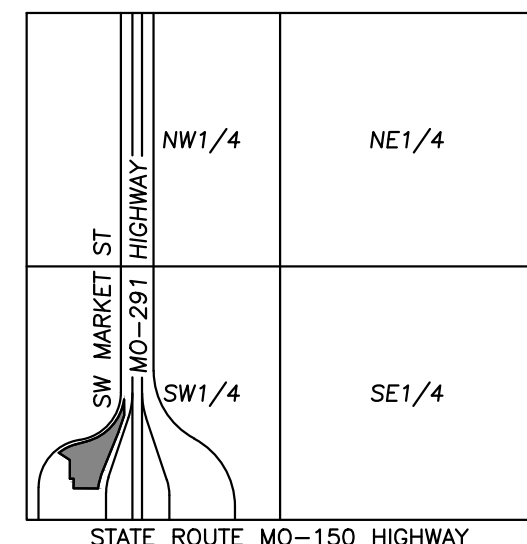


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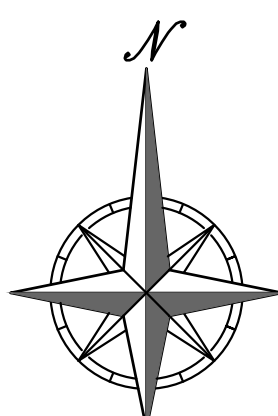
VERTICAL DATUM = NAVD88 BASED ON GPS OBSERVATION USING MODOT VRS
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ELEVATION = 987.14
2. SET "C" CUT IN SOUTHWEST CORNER OF BACK OF CURB IN ADJACENT PARKING LOT TO THE NORTH AT NORTHWEST CORNER OF SURVEYED PROPERTY.
ELEVATION = 990.19



VICINITY MAP
SEC. 29-47-31

LEGEND

— PL —	PROPERTY LINE
— LL —	LOT LINE
— R/W —	RIGHT-OF-WAY
— 2' CURB & GUTTER —	2' CURB & GUTTER
— 918 —	EXISTING CONTOURS
— 920 —	PROPOSED CONTOURS
— 918 —	PROPOSED SPOT ELEVATION
XXX.XX	LG LIP OF GUTTER
TW	TC TOP OF CURB
	SW SIDEWALK
	ME MATCH EXISTING
	HP HIGH POINT
	LP LOW POINT
	P TOP OF PAVEMENT
	TE TOP OF STRUCTURE
	GR GROUND ELEVATION
	BS BOTTOM OF STEPS
	TS TOP OF STEPS
	BW BOTTOM OF WALL
	TW TOP OF WALL
— — —	EXISTING STORM SEWER
— — —	PROPOSED STORM PIPE
— — —	PROPOSED WET CURB & GUTTER
— — —	PROPOSED DRY CURB & GUTTER
— — —	PROPOSED RETAINING WALL



SCALE: 1"=20'
0' 20' 40'

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**ENLARGED
GRADING
PLAN**

sheet number

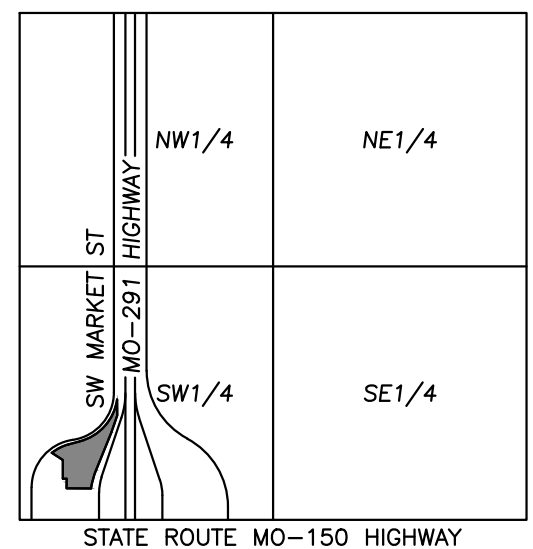
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drawing type

PDP

project number

22185



VICINITY MAP
SEC. 29-47-31



a new store for
Westlake ACE Hardware
3511 SW Market Street
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UTILITY
PLAN
sheet number
C3
drawing type
PDP
project number
22185

LOT 1
MARKET STREET COMMONS
LOTS 1-3 WAL-MART CORP

LOT 1
1.85 AC.
PREVIOUSLY APPROVED
& UNDER CONSTRUCTION

LOT 2
2.58 AC.

LOT 1, BLOCK 1
3561 SW MARKET ST
F'S LEES SUMMIT LLC

LOT 1A,
QUIKTRIP 200R LOT 1A
PIED CREEK MISSOURI LLC

LOT 2
QUIKTRIP 200R
COMMERCIAL FEDERAL BANK

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4.5X6 CURB INLET
T.E.=1009.83
E(S)8" PVC=1005.63

SSMH T.E.=1012.35
E(N)8" PVC=1006.27
E(W)8" PVC=1006.27
E(S)8" PVC=1005.10

3X6 CURB INLET
T.E.=1014.13
FL(E)36" HDPE=1001.34

FL(V)36" HDPE=999.64
FL(E)36" HDPE=999.47

CONCRETE PAVING

FOUND "+"
OUT IN PAVEMENT

36" HDPE W/
WING WALLS
FL=999.42

EXISTING
DETENTION BASIN

INSTALL
PRIVATE FH

TRACT A
0.60 AC.

STM

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date

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revisions

1. 09-13-22
REVISED PER PLANNING
COMMENTS

**STANDARD
DETAILS**

sheet number

C4

drawing type
PDP

project number
22185

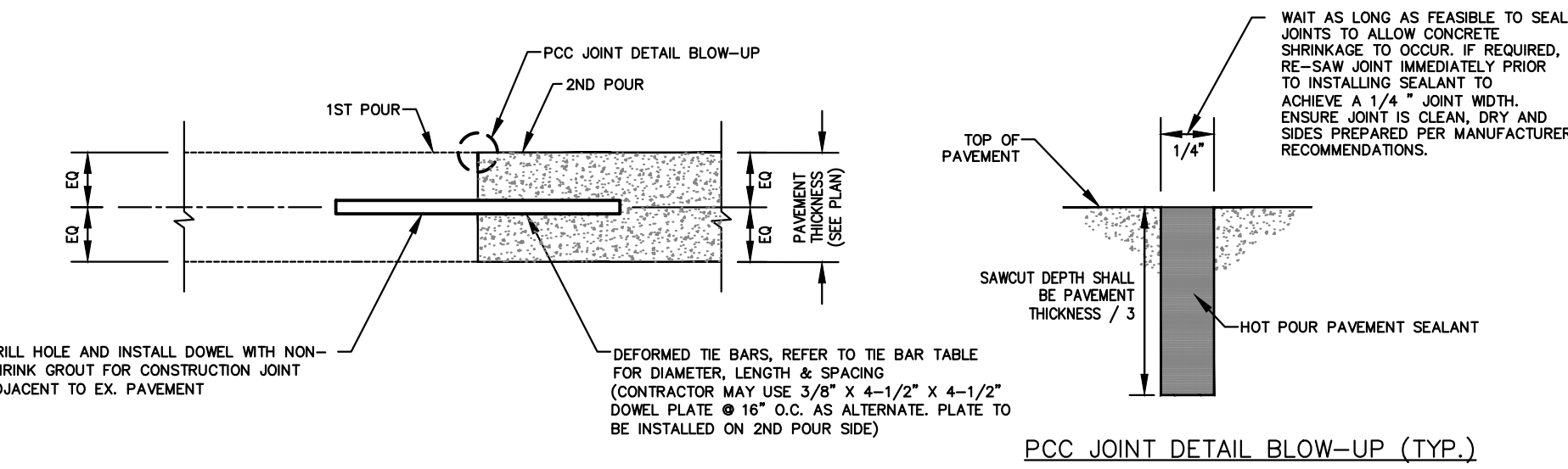
Dowel size*			
Slab depth, in. (mm)	Dowel diameter, in. (mm)	Dowel embedment, in. (mm) ¹	Total dowel length, in. (mm) ²
5 (125)	5/8 (16)	5 (125)	12 (300)
6 (150)	3/4 (19)	6 (150)	14 (360)
7 (180)	7/8 (22)	6 (150)	14 (360)
8 (200)	1 (25)	6 (150)	14 (360)
9 (230)	1-1/8 (29)	7 (180)	16 (400)

*All dowels spaced at 12 in. (300 mm) centers.

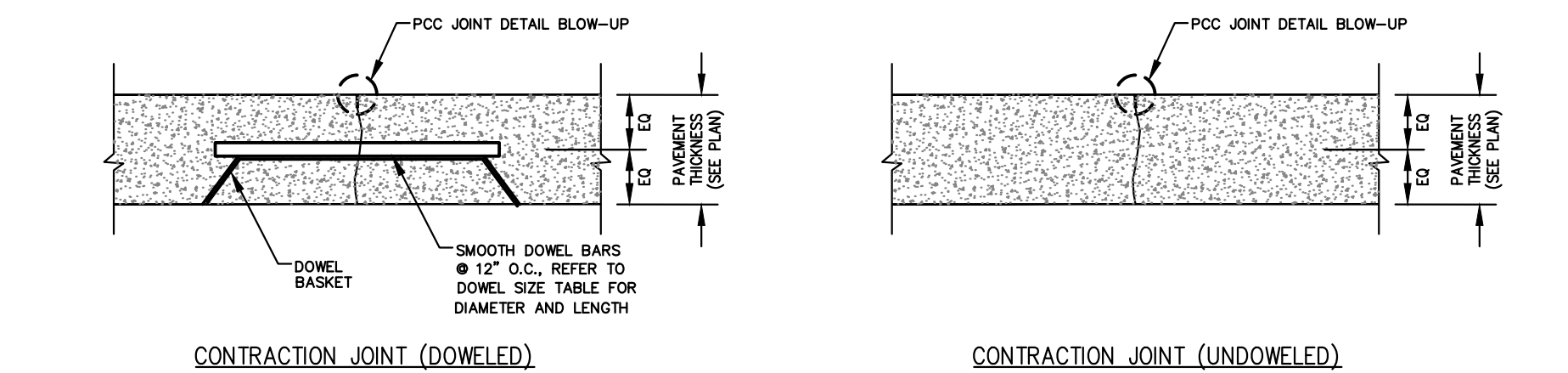
¹On each side of joint.

²Allowance made for joint openings and for minor errors in positioning dowels.

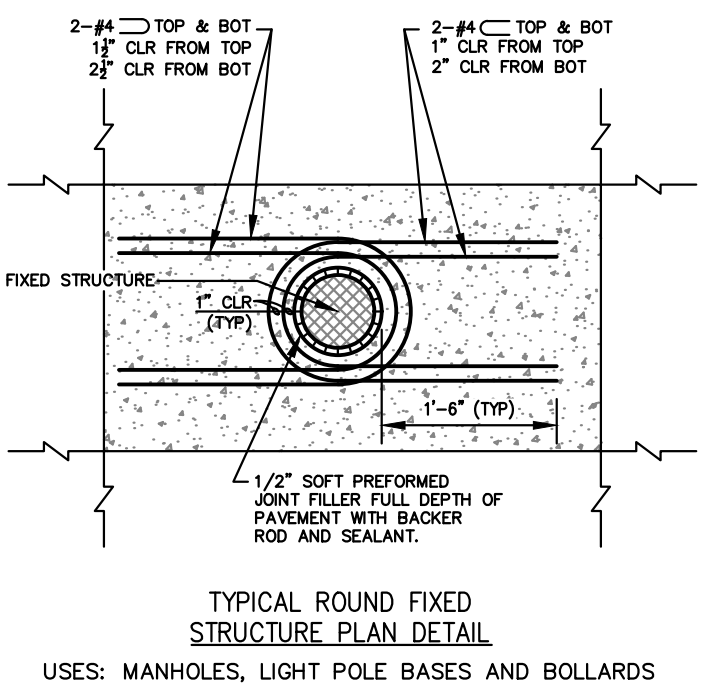
Tie bar dimensions		Tiebar spacing			
Slab depth, in. (mm)	Tiebar size, in. (mm)	Distance to nearest free edge or to nearest joint where movement can occur			
		10 ft. in. (mm)	12 ft. in. (mm)	14 ft. in. (mm)	24 ft. in. (mm)
5 (125)	1/2 x 24 (13 x 610)	30 (760)	30 (760)	30 (760)	28 (710)
5-1/2 (140)	1/2 x 24 (13 x 610)	30 (760)	30 (760)	30 (760)	25 (630)
6 (150)	1/2 x 24 (13 x 610)	30 (760)	30 (760)	30 (760)	23 (580)
6-1/2 (165)	1/2 x 24 (13 x 610)	30 (760)	30 (760)	30 (760)	21 (530)
7 (180)	1/2 x 24 (13 x 610)	30 (760)	30 (760)	30 (760)	20 (510)
7-1/2 (190)	1/2 x 24 (13 x 610)	30 (760)	30 (760)	30 (760)	18 (460)
8 (200)	1/2 x 24 (13 x 610)	30 (760)	30 (760)	28 (710)	17 (430)
8-1/2 (215)	1/2 x 24 (13 x 610)	30 (760)	30 (760)	36 (910)	16 (410)
9 (230)	1/2 x 30 (13 x 760)	36 (910)	36 (910)	—	24 (610)



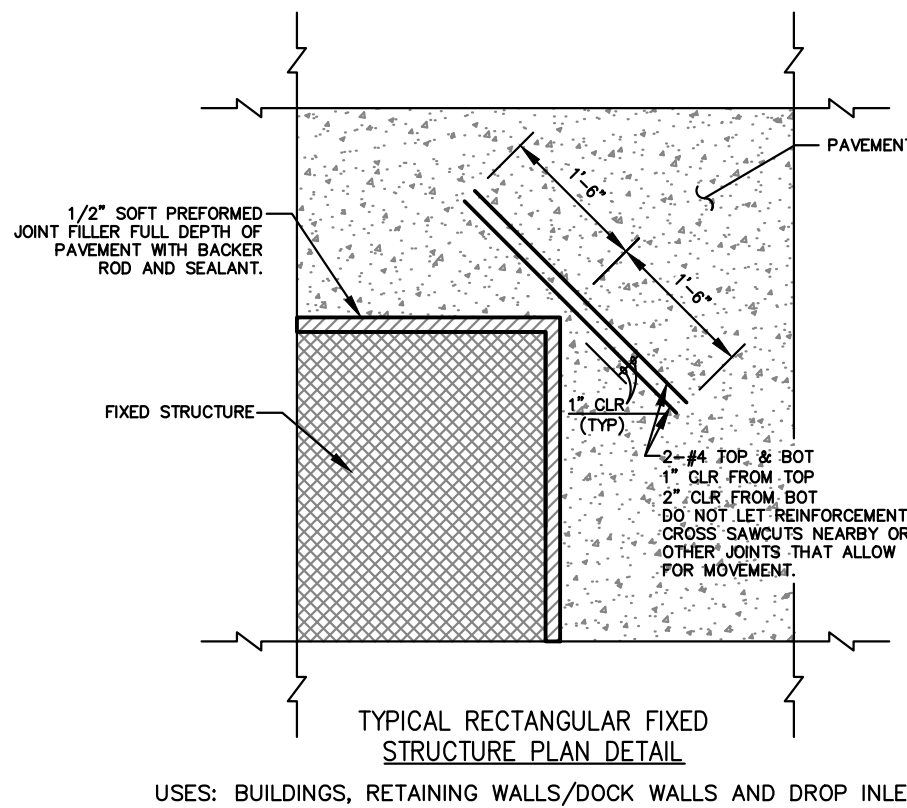
CONSTRUCTION JOINT



CONCRETE JOINT DETAILS
SCALE: N.T.S.

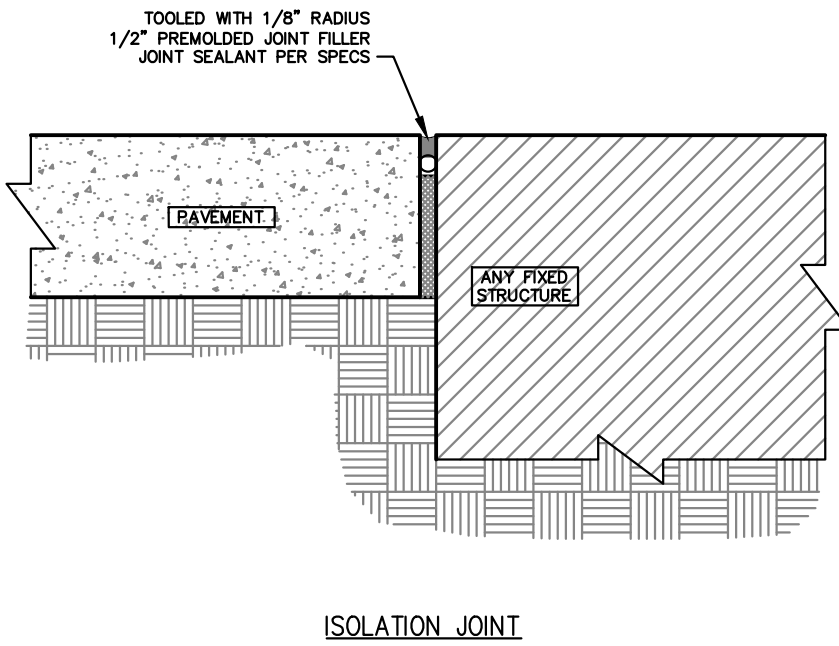
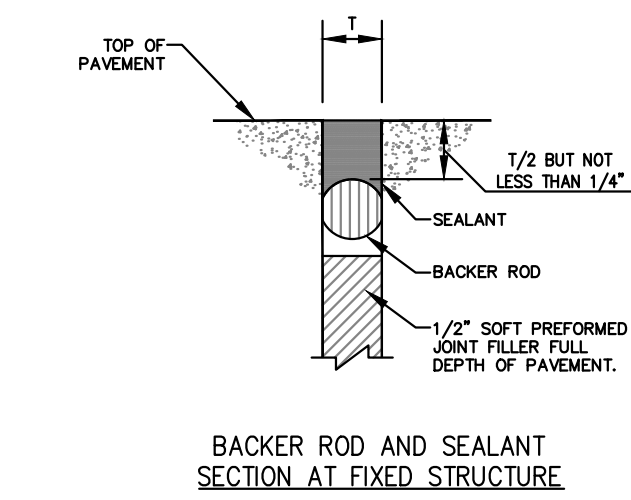


USES: MANHOLES, LIGHT POLE BASES AND BOLLARDS



USES: BUILDINGS, RETAINING WALLS/DOCK WALLS AND DROP INLETS

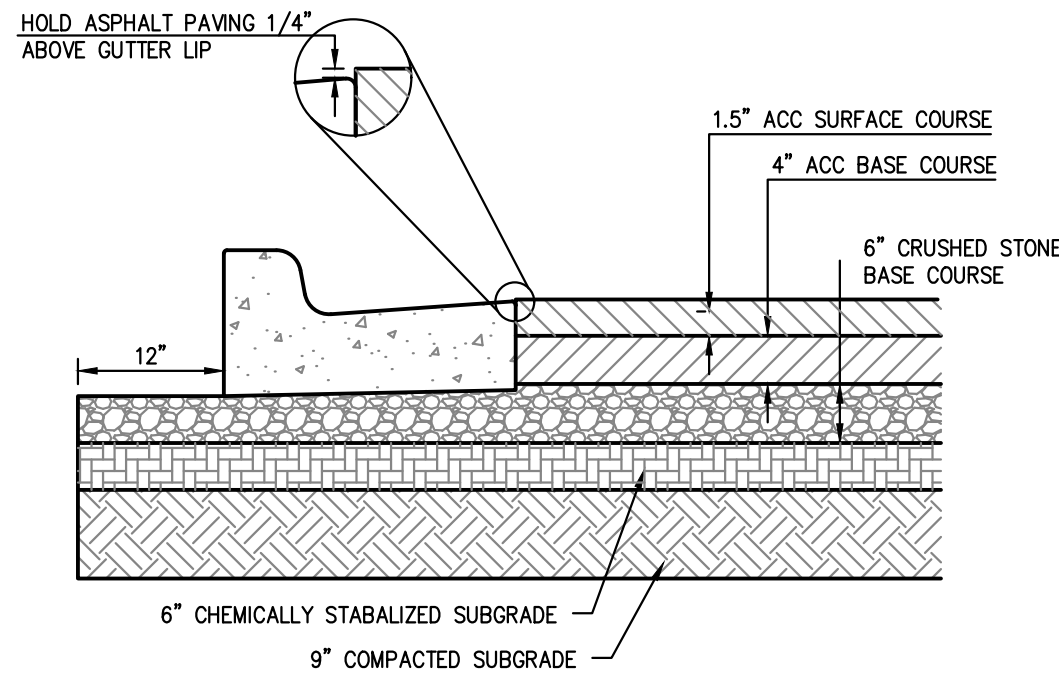
ISOLATION JOINT DETAILS
SCALE: N.T.S.



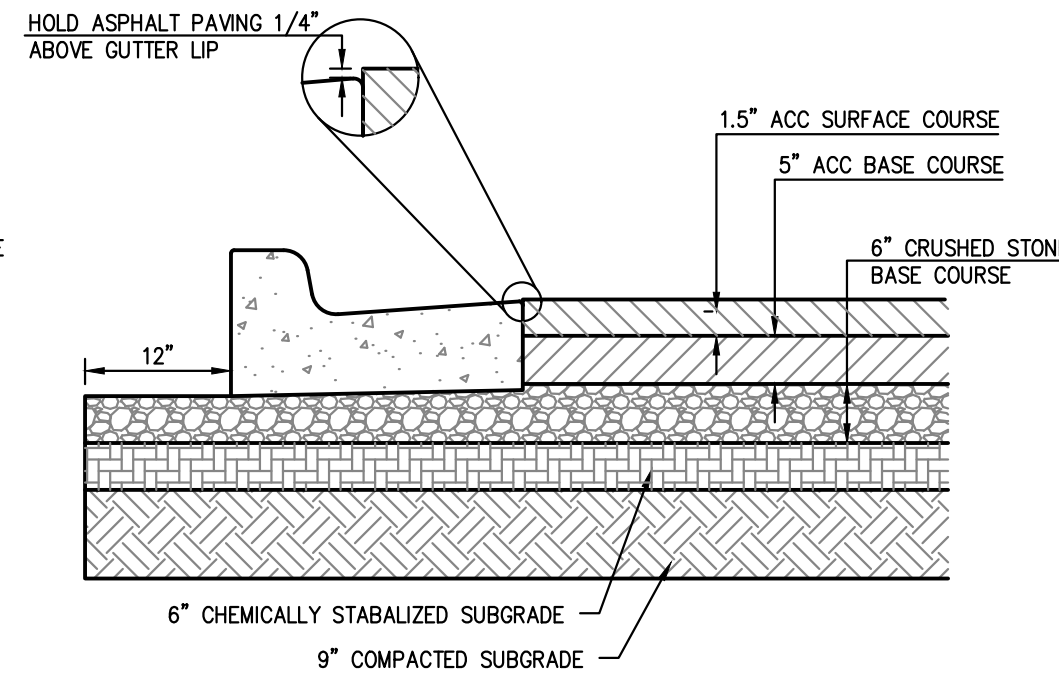
NOTES:
ISOLATION JOINT TO BE USED FOR FIXED STRUCTURES
SUCH AS BUILDINGS, RETAINING WALLS/DOCK WALLS,
DROP INLETS, MANHOLES, LIGHT POLE BASES AND
BOLLARDS.
PAVEMENT IS NOT CONSIDERED A FIXED STRUCTURE.

GENERAL PAVING NOTES:

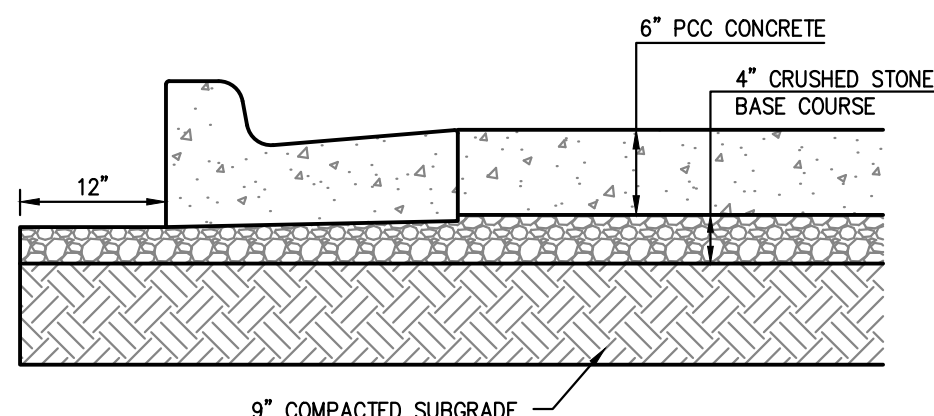
- PRIOR TO PLACEMENT OF GRANULAR BASE OR ASPHALT, PROOF ROLL AND RE-COMPACT THE EXPOSED SURFACES UP TO A MINIMUM LATERAL DISTANCE OF TWO (2) FEET OUTSIDE THE PAVEMENT. ANY LOCALIZED SOFT, WET, OR LOOSE AREAS IDENTIFIED DURING THE PROOF ROLLING SHOULD BE REPAIRED PRIOR TO PAVING. FILL MATERIAL SHOULD BE PLACED IN LOOSE LIFTS UP TO A MAXIMUM OF EIGHT (8) INCHES IN THICKNESS AND COMPACTED TO AT LEAST 95% OF THE MAXIMUM DRY DENSITY IN ACCORDANCE WITH ASTM D698 AT MOISTURE CONTENTS WITHIN 0% AND +4% OF THE OPTIMUM FOR SOILS WITH A LIQUID LIMIT OF GREATER THAN 40, AND - +/- 3% OF THE OPTIMUM FOR SOILS WITH A LIQUID LIMIT OF LESS THAN 40. MAXIMUM DRY DENSITY AND OPTIMUM MOISTURE CONTENT SHOULD BE DETERMINED BY THE STANDARD PROCTOR TEST (ASTM D 698).
- PROOFROLL WITH A 25 TON RUBBER TIRE VEHICLE AND REPAIR SUBGRADE DEFICIENCIES. IF ANY SIGNIFICANT EVENT, SUCH AS PRECIPITATION, OCCURS AFTER PROOFROLLING, THE SUBGRADE SHOULD BE REVIEWED BY QUALIFIED PERSONNEL IMMEDIATELY PRIOR TO PLACING THE PAVEMENT.
- CRUSHED STONE BASE COURSE USED BENEATH CONCRETE PAVING SHALL BE COMPACTED KDOT AB-3 OR EQUIVALENT.
- ASPHALTIC SURFACE COURSE SHALL BE APWA TYPE 3. THE SURFACE COURSE SHOULD BE COMPACTED TO A MINIMUM OF 97% MARSHALL DENSITY (ASTM SPECIFICATION D 1559). 30% RAP IS ALLOWED.
- ASPHALTIC BASE COURSE SHALL BE APWA TYPE 1. THE BASE COURSE SHOULD BE COMPACTED TO A MINIMUM OF 95% MARSHALL DENSITY (ASTM SPECIFICATION D 1559). 30% RAP IS ALLOWED.
- THE CONTRACTOR SHALL PROVIDE A TACK COAT BETWEEN LIFTS OF ASPHALT.
- ALL SITE CONCRETE (CURBS, PAVEMENTS, SIDEWALKS, ETC.) SHALL MEET KANSAS CITY MATERIALS METRO BOARD (KCMMB) MIX DESIGN SPECIFICATIONS FOR 4,000 P.S.I. AIR ENTRAINED CONCRETE.
- IN NEW PAVEMENT AREAS, CONTRACTOR SHALL OVER EXCAVATE AS REQUIRED TO ESTABLISH NEW COMPACTED SUBGRADE ELEVATIONS.
- CONTRACTOR IS RESPONSIBLE FOR ALL PAVEMENT AND SUBGRADE MATERIALS TESTING.



STANDARD ASPHALT PAVING

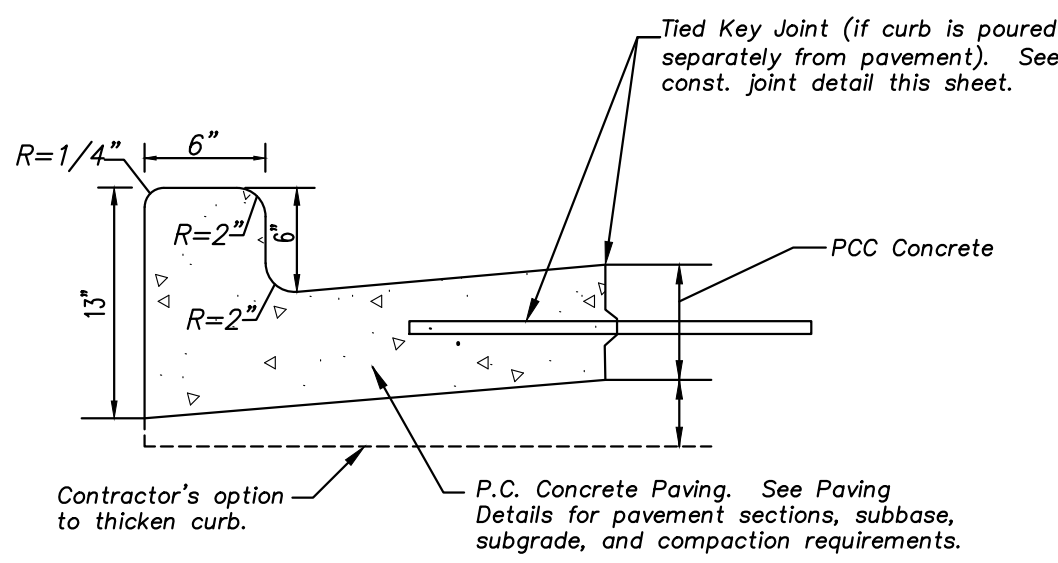


HEAVY DUTY ASPHALT PAVING

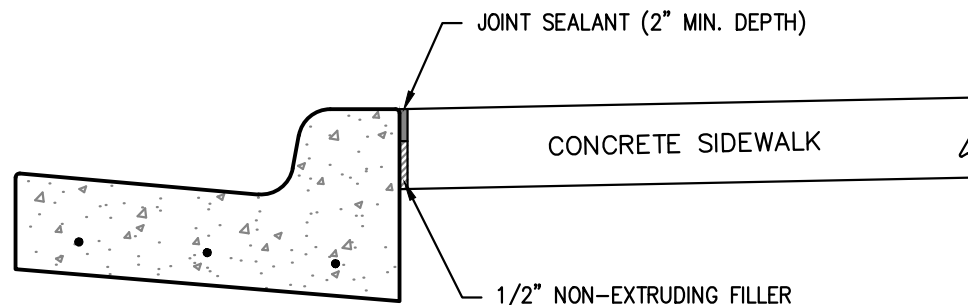


CONCRETE PAVING

PAVING SECTIONS
SCALE: N.T.S.

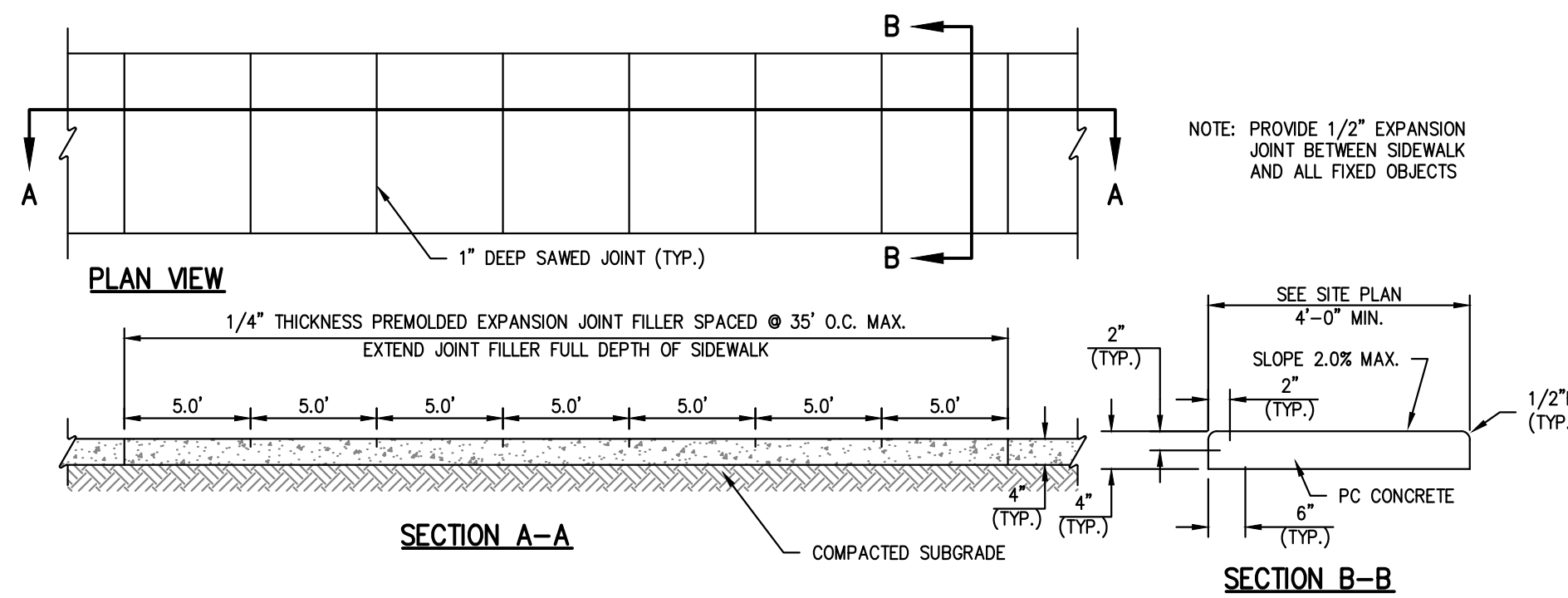


MONOLITHIC CONCRETE CURB DETAIL
SCALE: N.T.S.



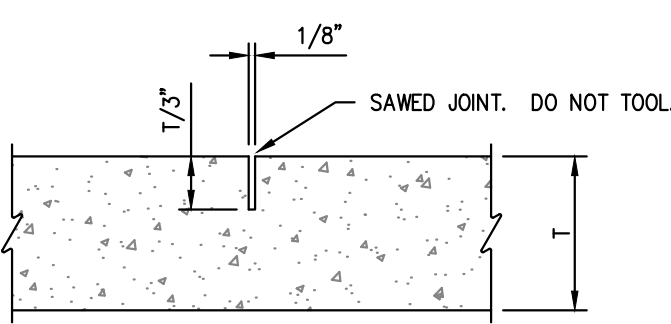
ALL OTHER DETAILS SAME AS SHOWN PER THIS SHEET.

SIDEWALK AT CURB DETAIL
SCALE: N.T.S.

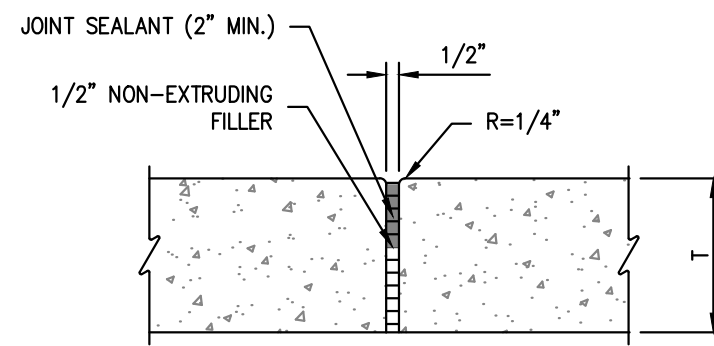


NOTE:
1. USE KANSAS CITY MATERIALS METRO BOARD (KCMMB) MIX DESIGN SPECIFICATIONS FOR 4,000 P.S.I. AIR ENTRAINED CONCRETE FOR ALL PRIVATE SIDEWALKS.

PRIVATE CONCRETE SIDEWALKS (NON REINFORCED)
SCALE: N.T.S.



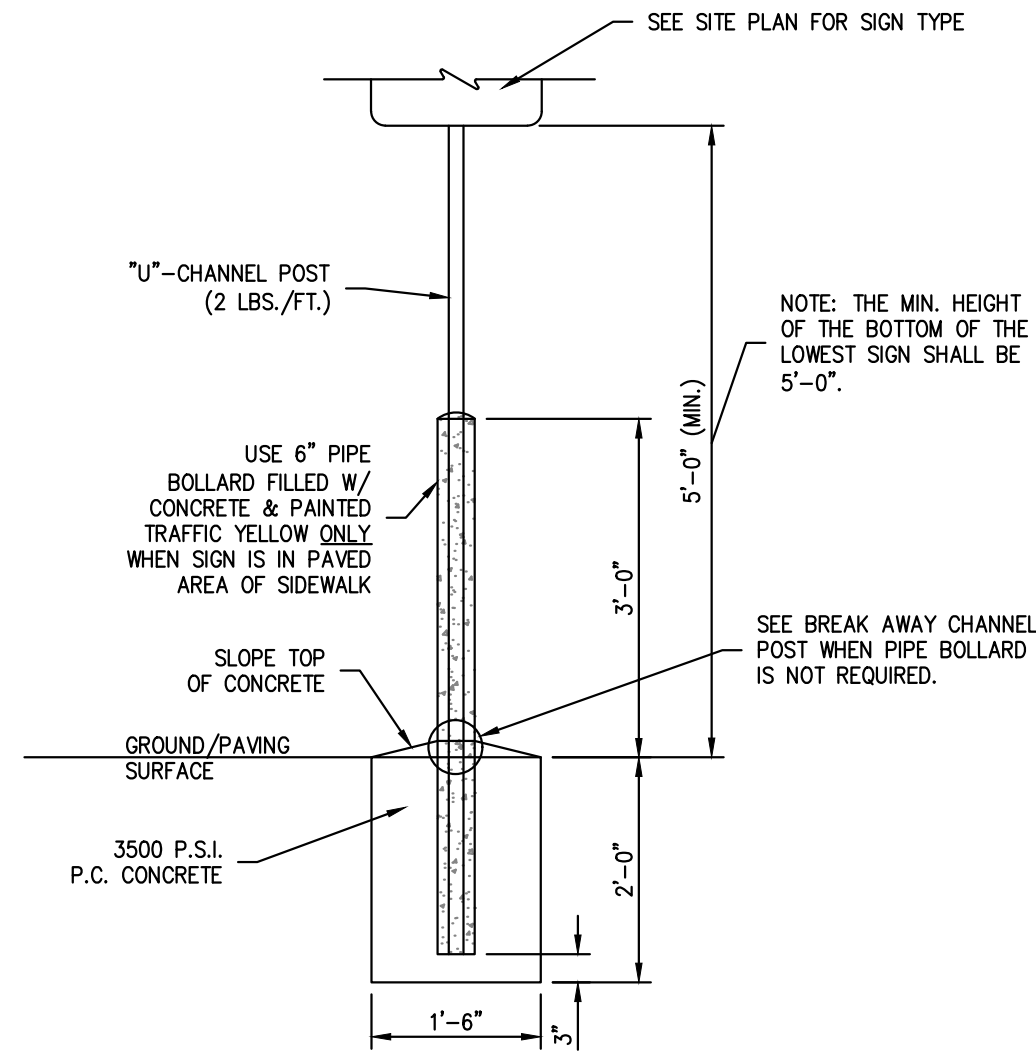
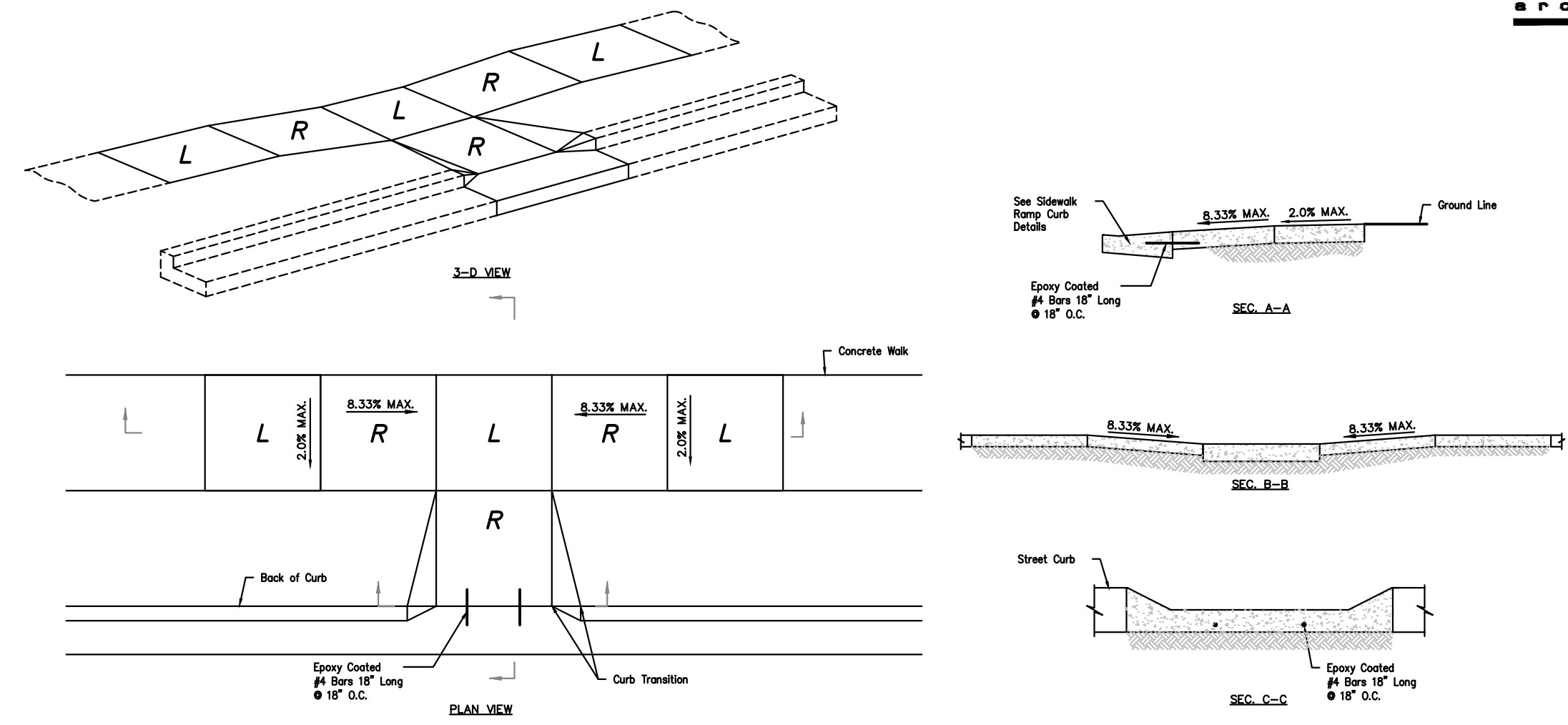
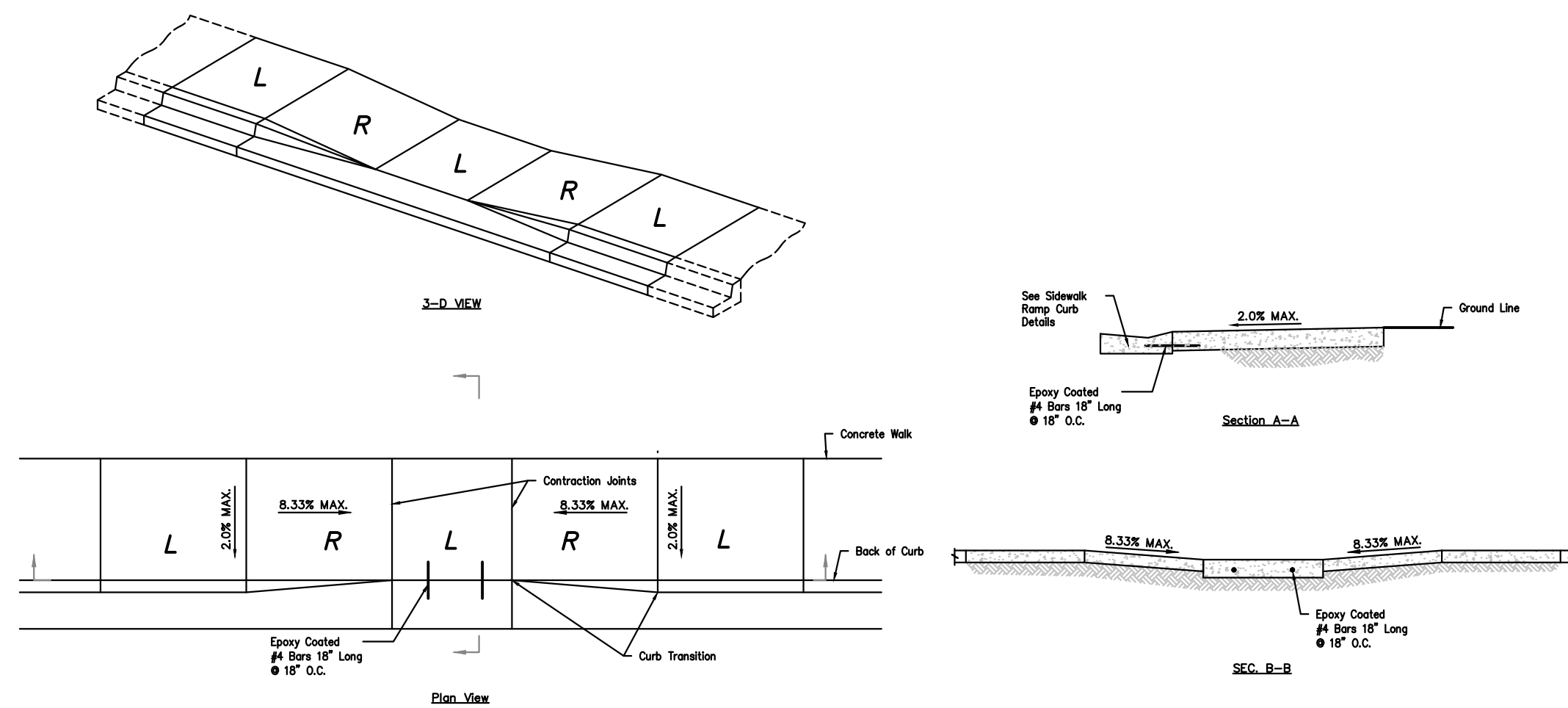
TYPE A JOINT



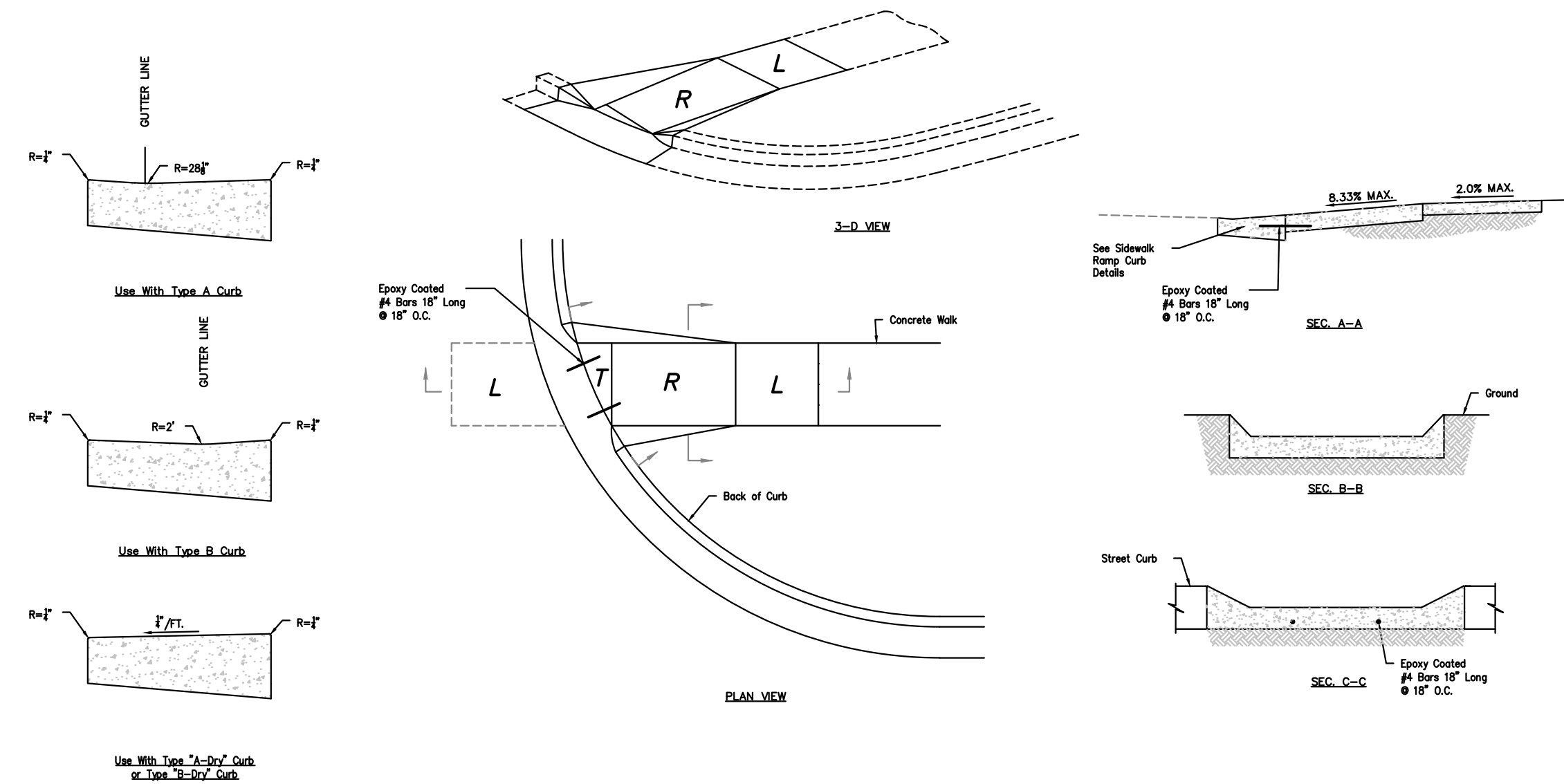
TYPE B JOINT

NOTE: TYPE A JOINTS SHALL NOT EXCEED 20 TIMES THE PAVEMENT THICKNESS (T).

CONCRETE SIDEWALK JOINT DETAILS
SCALE: N.T.S.



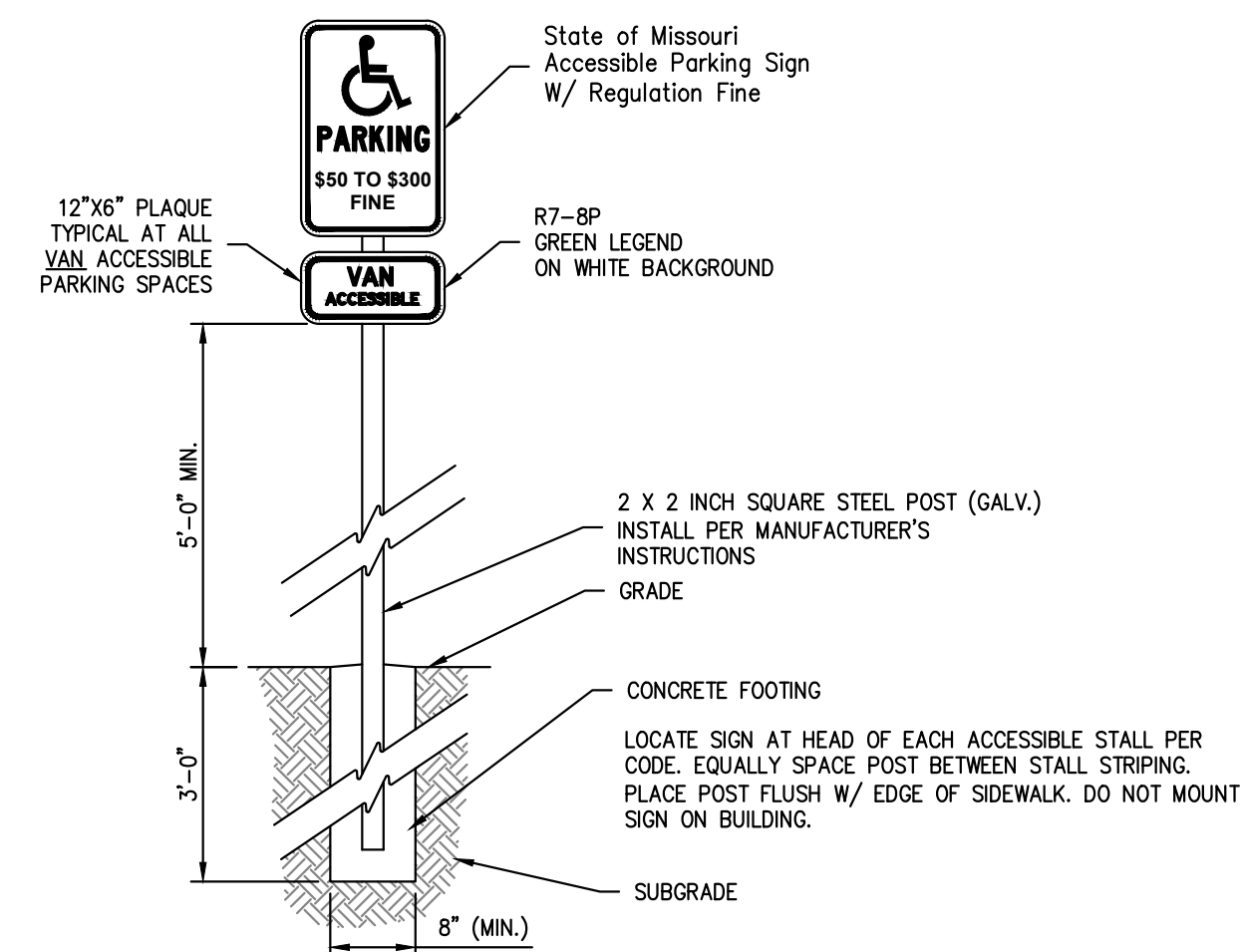
**SIGN BASE DETAIL IN
SIDEWALK & PAVED AREAS**
SCALE: N.T.S.



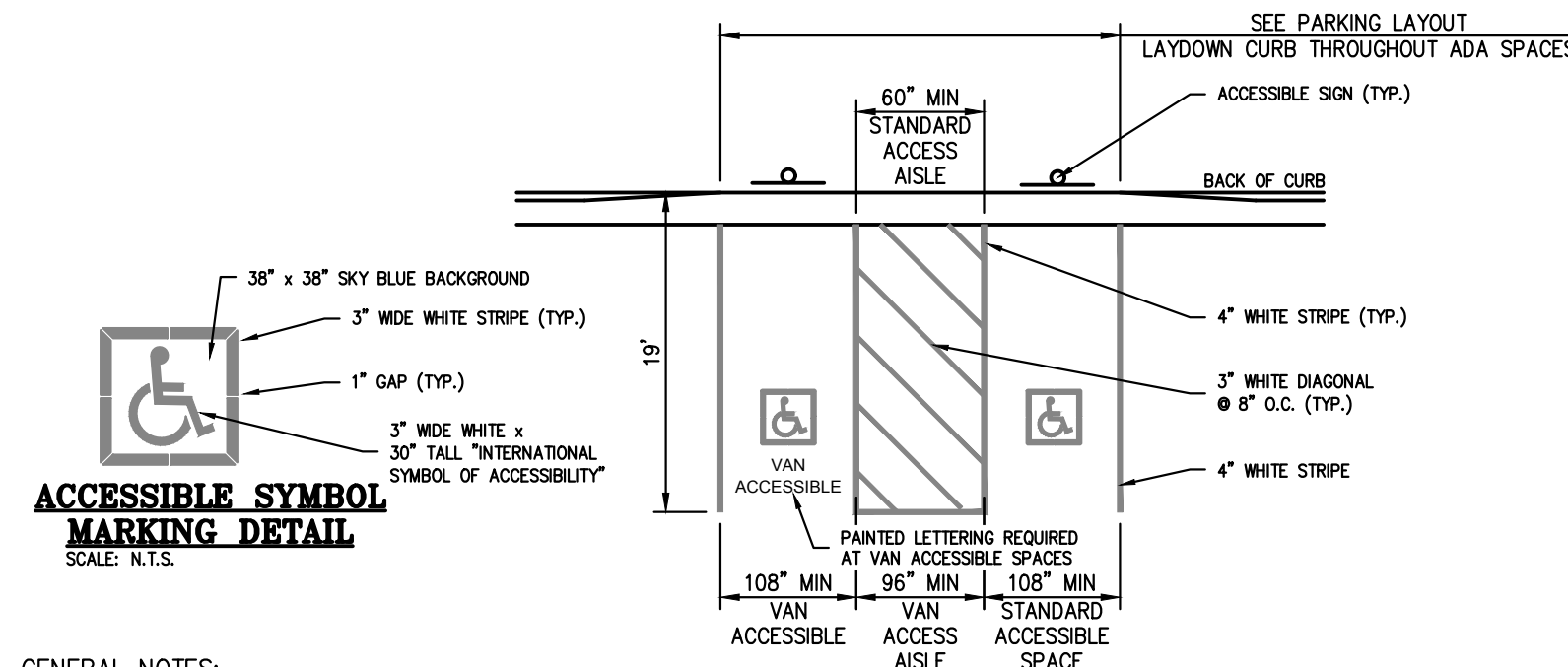
L = LANDING
R = RAMP
T = TRANSITION

RAMP (Required to transition elevation): Max. Longitudinal Slope = 0.33%
Max. Cross Slope = 2.00%
Min. Width = 5'
Min. Length = 5'
LANDING (Required to change direction of travel): Max. Longitudinal Slope = 2.00%
Max. Cross Slope = 2.00%
Min. Width = 5'

PRIVATE SIDEWALK RAMPS
SCALE: N.T.S.



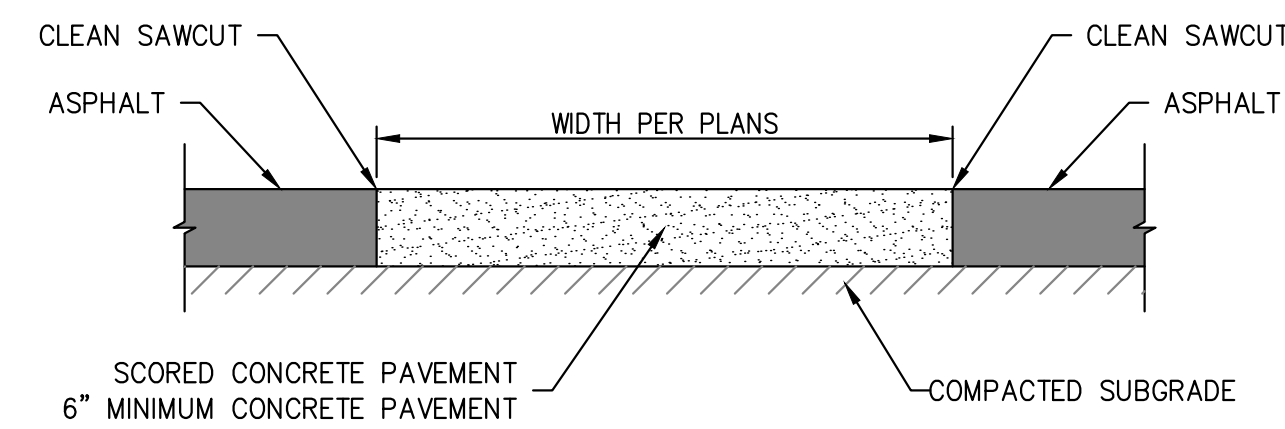
**ACCESSIBLE SIGN DETAIL
IN GRASS AREA**
SCALE: N.T.S.



GENERAL NOTES:

1. ALL PAVEMENT MARKINGS SHALL BE APPLIED BY A QUALIFIED CONTRACTOR HAVING A MINIMUM 3 YEARS EXPERIENCE IN TRAFFIC GRADE PAVEMENT MARKING APPLICATIONS.
2. PAINT SHALL BE A NON-BLEEDING, QUICK-DRYING, ALKYL PETROLEUM BASE PAINT SUITABLE FOR TRAFFIC-BEARING SURFACE AND SHALL MEET FS TYP-BSE & MIXED IN ACCORDANCE WITH THE MANUFACTURER'S INSTRUCTIONS BEFORE APPLICATION.
3. SWEEP AND CLEAN SURFACE TO ELIMINATE LOOSE MATERIAL & DUST.
4. APPLY TWO (2) COATS OF PAINT AT MANUFACTURER RECOMMENDED RATE WITHOUT THE ADDITION OF THINNER, WITH A MAXIMUM OF 100 SQUARE FEET PER GALLON. APPLY WITH MECHANICAL EQUIPMENT TO PRODUCE UNIFORM STRAIGHT EDGES. AT SIDEWALK, CURBS, AND CROSSWALKS USE A STRAIGHTEDGE TO ENSURE A UNIFORM, CLEAN, & STRAIGHT STRIPE.
5. THE FOLLOWING ITEMS SHALL BE PAINTED WITH THE COLORS NOTED BELOW:
A. HANDICAP SYMBOLS: SEE DETAIL THIS SHEET.
B. PARKING STALL STRIPING: WHITE.
6. ACCESSIBLE PARKING SPACE DESIGN LAYOUT SHALL BE IN ACCORDANCE WITH CURRENT ADA REQUIREMENTS.
7. SEE SITE PLANS FOR COMPLETE PARKING LAYOUT.

ACCESSIBLE PARKING SPACE DETAIL
SCALE: N.T.S.



CROSSWALK DETAIL
SCALE: N.T.S.

\\PHILIPS-SERVER\Projects\PA\220695\Draw\Preliminary\DETAILS - PRIVATE.dwg LayoutPAVE 3 Sep 13, 2022 2:43pm Daniel Finn

a new store for
Westlake ACE Hardware
3511 SW Market Street
Lee's Summit, Missouri 64082

date

drawn by

checked by

revisions

1. 09-13-22
REVISED PER PLANNING
COMMENTS

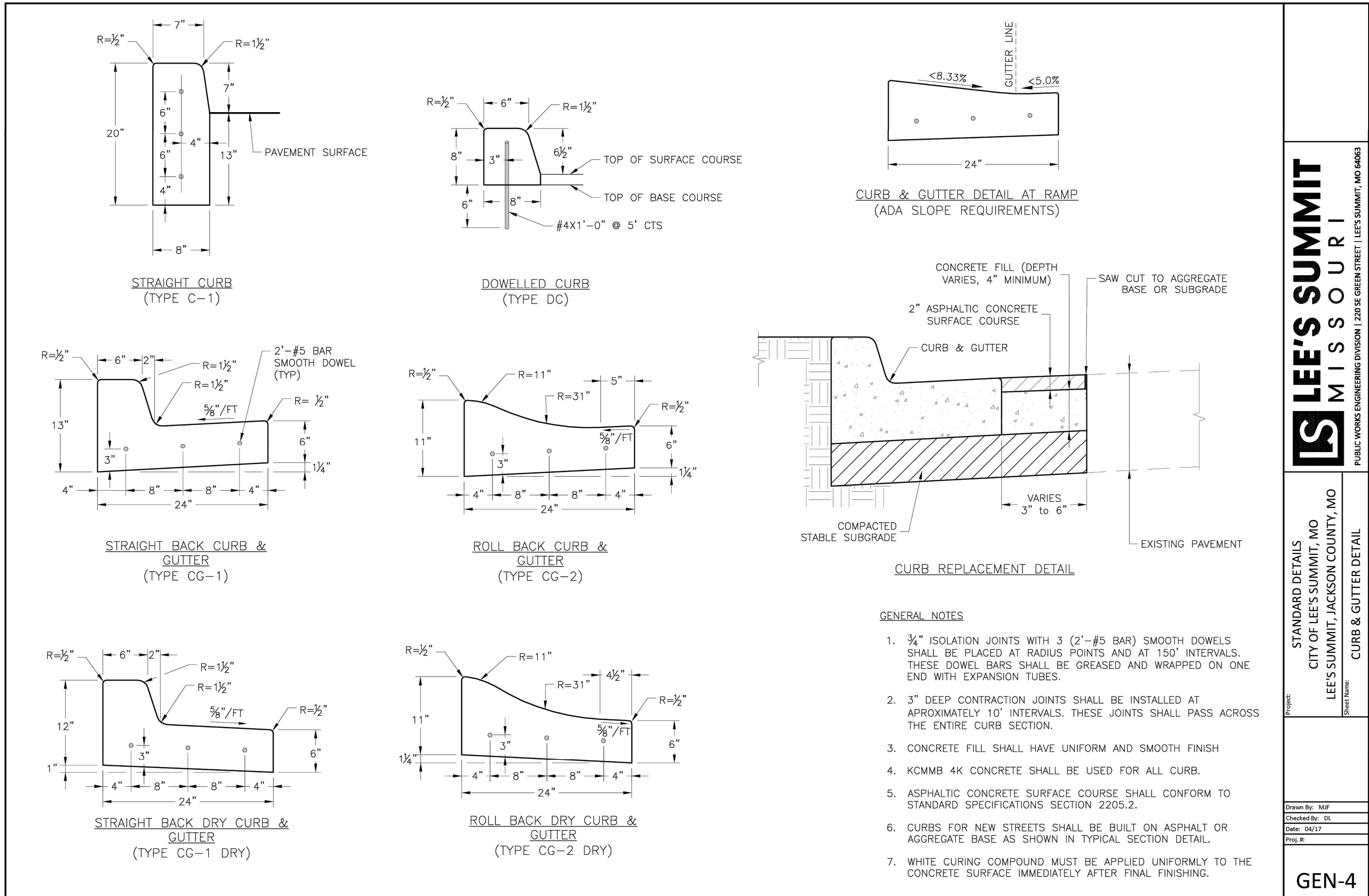
**STANDARD
DETAILS**

sheet number

C4.2

drawing type
PDP

project number
22185



LEE'S SUMMIT
MISSOURI

STANDARD DETAILS
CITY OF LEE'S SUMMIT, MO
LEE'S SUMMIT, JACKSON COUNTY, MO
Sheet Name

GEN-4

LANDSCAPE REQUIREMENTS:

	Required	Provided
Street Trees 1 tree per 30', 1 shrub per 20' (263lf)	9	9
SW Market Street Trees	13	14
Shrubs		

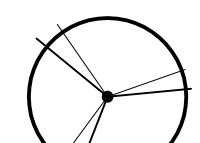

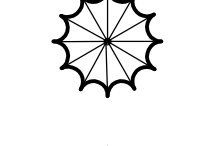
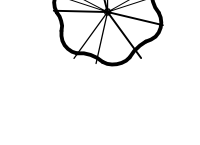


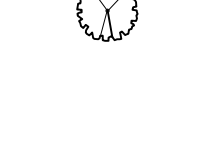

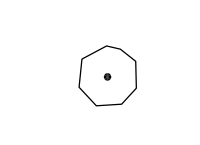

Open Yard 97,104 s.f.		
2 Shrubs per 5000s.f.	39	51
1 Tree per 5000s.f.	19.4	28

Screening Parking Lot		
Market Street (182 lf)		
12 shrubs per 40 l.f.	54.6	55

Hwy. 291 (188 lf)		
12 shrubs per 40 l.f.	56.4	*31

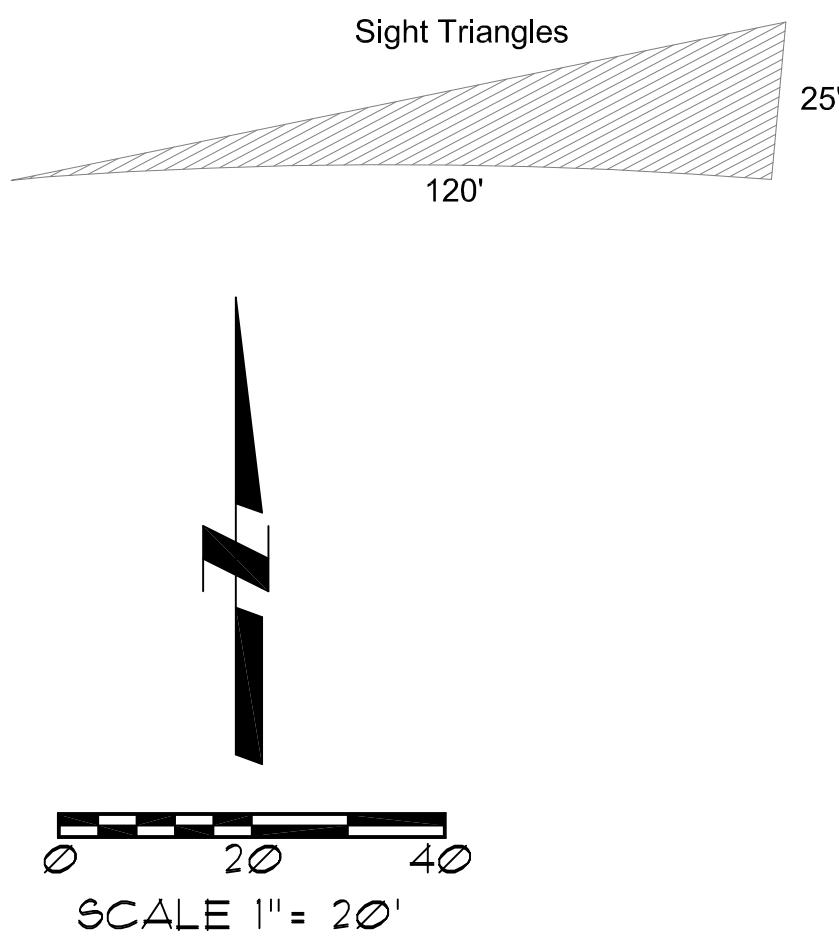
*11-8ft Evergreen trees substituted for 25 shrubs

CONCEPT PLANT SCHEDULE

	LARGE SHADE TREES 3" CAL.	19
	MEDIUM SHADE TREES 3" CAL.	12
	EVERGREEN TREES 8 FT.	14
	ORNAMENTAL TREE 3" CAL.	2
	SPREADER EVERGREEN SHRUB Seagreen Juniper 24"-30"	45
	DECIDUOUS SHRUB I Grow Low Sumac 18"-24"	24
	DECIDUOUS SHRUB II Centerglow Ninebark, Tiger Eyes Sumac, Prairie Fire Sumac 24"-30"	24
	LOW SPREADING EVERGREEN Grey Owl Juniper 18"-24"	28
	ORNAMENTAL GRASS TALL Feathered Grass 24"-30"	3
	ORNAMENTAL GRASS TALL II	21

Utility Note:

Utilities shown on plan are diagrammatic and some may be missing. Before starting any construction call appropriate locating service. In Missouri call 1-800-DIG-RITE (344-7483) to have utilities located.



a new store for
Westlake ACE Hardware
3511 SW Market Street
Lee's Summit, Missouri 64082

date 09/08/2022

drawn by PAO

checked by PAO

revisions

sheet number

LS-1

drawing type

Preliminary Development

Landscape Plan

project number

22185



Oppermann LandDesign, LLC
Land Planning Landscape Architecture
22 Debra Lane pete@oppermans5@gmail.com
New Windsor, New York 12553 913.529.5598



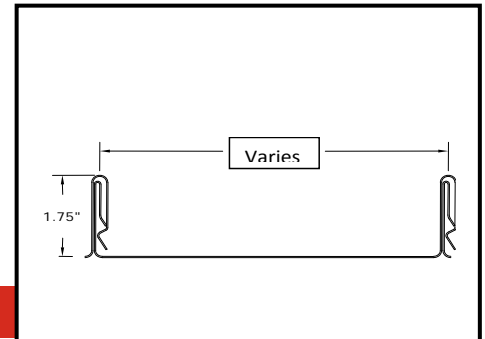
Example of standing seam metal fascia

TECHNICAL INFORMATION SHEET

UNA-CLAD™ UC-14

Item Description

Standing Seam Panel For Architectural Metal Roofing



Product Information

Description:

Firestone UNA-CLAD UC-14 Roofing Panel is a factory formed continuous interlocking, concealed clip, architectural standing seam metal roof panel with a proven seam design that snaps together for ease of installation. An optional thermally-applied pre-assembly in-seam sealant is available. The minimum slope requirement for a Firestone Red Shield™ Warranty is 3:12. For warranty requirements for slopes between 3:12 and 1:12, please contact your Building Systems Advisor.

Method of Application:

1. Firestone UC-14 panels must be installed in a sequential order.
2. Application of a Firestone underlayment prior to panel installation is recommended.
3. A smooth, solid substrate of plywood, OBS, or a rigid insulation board mechanically attached to a steel deck is recommended for the Firestone UC-14 metal roof panel.

NOTE: Install assembly according to Firestone Metal Design and Application Guides found on the Firestone website. Follow approved installation details.

Storage:

- Firestone metal panels should be stored in a well ventilated, dry place where no moisture can contact them. Moisture (From rain, snow, condensation, etc.) trapped between layers of material may cause water stains or white rust, which can affect the service life of the material and will detract for the appearance.
- If outdoor storage cannot be avoided, protect the panels with a ventilated canvas or waterproof paper cover. Do not use plastic, which can cause condensation. Keep the material off the ground in an inclined position with an insulator such as wood. Protective film may degrade or become brittle with long term exposure to direct sunlight.

Precautions:

- Oil canning is not a cause for rejection. Heavier gauges, narrower widths, striations, and embossing minimize oil canning.
- Firestone recommends a minimum bend radius of 2T. Anything less than a 2T bend radius can cause crazing to the material.
- Sealant for end laps and lap joints shall be non-drying, non-toxic, and non-shrinking with a serviceable temperature of -60 to 212 °F (-51 to 100 °C).
- Quality, long-life butyl sealants work best as a gasket sandwiched between two pieces of metal. Non-acetic cured silicone color matching sealants are recommended when voids must be filled. Sealants are not a substitute for proper assembly and workmanship.
- Exercise caution when lifting, moving, transporting, storing or handling Firestone metal to avoid possible physical damage.
- Refer to Safety Data Sheets (SDS) for safety information.
- Immediately remove protective film after installation.

TECHNICAL INFORMATION SHEET

UNA-CLAD™ UC-14

Manufacturing Location:

Anoka, MN



Product Data

Tapered Panels:	No
Radius Panels:	No
Stiffening Ribs:	Optional
Striations:	Optional
Sealant:	Optional In-Seam, Thermally Applied
Standard Panel Surface:	Smooth
Optional Panel Surface:	Stucco Embossed
Clip:	UC-14 Clip

Product Size

Panel Width:	8" (203.2 mm) – 24" (609.6 mm)
Optimal Panel Width:	18" (457.2 mm)
Seam Height:	1.75" (44.5 mm)
Minimum Panel Length:	36" (914.4 mm)
Maximum Panel Length:	600" (15.24 m)

Technical Information

Up lift Resistance:	UL 580 Class 90
Air Infiltration:	ASTM E 1680
Uniform Static Air Pressure :	ASTM E 1592
Water Penetration:	ASTM E 1646
Fire Rating:	UL Class A Rated Assemblies, UL 263 and UL 790
Hail Impact Rating:	Class 4, UL 2218
Miami-Dade County & Florida Building Code:	Approved

NOTE: Testing is not applicable for all combinations of substrates, materials, and dimensions. All construction assemblies must be installed in accordance with the tested assembly. Please refer to the Metal Code Approval Guide on the Firestone website for tested assemblies and code listings.

Please contact your Building Systems Advisor for warranty requirements and additional information.



Metal Systems Color Availability

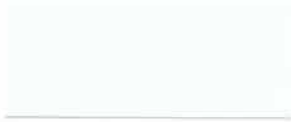
UNA-CLAD™ METAL PANEL COLOR SELECTION GUIDE



Stone White



Slate Gray



Bone White



Cityscape



Almond



Charcoal Gray



Sandstone



Sierra Tan



Medium Bronze



Dark Bronze



Extra Dark Bronze



Matte Black



Brandywine



Colonial Red



Terra Cotta



Mansard Brown



Regal Red



Award Blue



Sky Blue



Electric Blue



Regal Blue



Teal



Patina Green



Dark Ivy



Sherwood Green



Hartford Green



Hemlock Green



Tropical Patina



Silver Metallic



Classic Copper



Champagne Metallic

Firestone
BUILDING PRODUCTS
NOBODY COVERS YOU BETTER.™

Colors shown are as close to actual painted steel as allowed by the printing process. Contact your local sales rep for actual metal samples.