



LEE'S SUMMIT
MISSOURI
Development Services Department

Development Services Staff Report

File Number	PL2023-151
File Name	PRELIMINARY DEVELOPMENT PLAN – Streets of West Pryor, Lots 11 and 13
Applicant	Streets of West Pryor, LLC, applicant
Property Address/Location	1000 and 1020 NW Pryor Rd.
Planning Commission Date Heard by	August 24, 2023 Planning Commission and City Council
Analyst	Hector Soto, Jr., AICP, Senior Planner

Public Notification

Pre-application held: May 30, 2023

Neighborhood meeting conducted: July 10, 2023

Newspaper notification published on: August 5, 2023

Radius notices mailed to properties within 300 feet on: August 2, 2023

Site posted notice on: August 7, 2023

Table of Contents

1. Project Data and Facts	2
2. Land Use	3
3. Project Proposal	3
4. Unified Development Ordinance (UDO)	5
5. Comprehensive Plan	5
6. Analysis	6
7. Recommended Conditions of Approval	15

Attachments

Transportation Impact Analysis prepared by Erin Ralovo, PE, dated August 14, 2023 – 2 pages

Traffic Impact Study Update Memo prepared by Affinis, dated July 27, 2023 – 6 pages

Preliminary Development Plan, signed and sealed July 25, 2023 – 8 pages

Waterway (Lot 11) Building Elevations, dated August 17, 2023 – 3 pages

Waterway (Lot 11) Renderings, dated August 17, 2023 – 9 pages

Multi-tenant (Lot 13) Building Elevations, dated July 21, 2023; and Sheet A-901, dated August 4, 2023 – 5 pages

Lot 13 Photometric Plan, dated August 3, 2023

Location Map

1. Project Data and Facts

Project Data	
Applicant/Status	Streets of West Pryor, LLC / Applicant
Applicant's Representative	Dave Olson
Location of Property	1000 and 1020 NW Pryor Rd.
Size of Property	± 2.55 acres (111,008 sq. ft.) – Lot 11 ± 3.71 acres (161,760 sq. ft.) – Lot 13 ± 6.26 acres (272,768 sq. ft.) total
Number of Lots	2 lots
Number of Buildings	1 building + gas pump island canopy – Lot 11 1 multi-tenant building – Lot 13
Building Size	9,347 sq. ft. – Waterway Carwash (Lot 11) 12,000 sq. ft. – Multi-tenant building (Lot 13) 21,347 total sq. ft.
FAR (Floor Area Ratio)	0.08 FAR – Lot 11 0.07 FAR – Lot 13
Zoning	PMIX (Planned Mixed Use district)
Comprehensive Plan Designation	Summit Activity Center
Procedure	The Planning Commission makes a recommendation to the City Council on the proposed preliminary development plan. The City Council takes final action on the preliminary development plan. Duration of Validity: Preliminary development plan approval by the City Council shall not be valid for a period longer than twenty-four (24) months from the date of such approval, unless within such period a final development plan application is submitted. The City Council may grant one extension not exceeding twelve (12) months upon written request.

Current Land Use
The subject properties consist of a total of 6.26 acres of undeveloped commercial pad sites located at 1000 and 1020 NW Pryor Rd.

Description of Applicant's Request
The applicant proposes a 9,347 sq. ft. dual-tunnel carwash plus gas pump island canopy and a 12,000 sq. ft. multi-tenant restaurant building on Lots 11 and 13 of Streets of West Pryor, respectively. The

preliminary development plan approved in 2019 for the Streets of West Pryor identified the proposed Lot 11 as the site of a 7,500 sq. ft. sit-down restaurant. The approved plan identified the proposed Lot 13 as the site of a hotel, which the applicant will switch with the intended location for a sit-down restaurant. The proposed change in use for Lot 11 from a sit-down restaurant to a car wash plus the deviation in building architecture for both proposed buildings from the typical architectural aesthetic approved as part of the 2019 plan requires new preliminary development plan approval. Also included in this application is consideration of the applicant’s request to allow a greater number of signs than allowed under the sign ordinance.

2. Land Use

Description and Character of Surrounding Area

The subject properties are undeveloped pad sites at the northeast corner of the Streets of West Pryor development. Lot 11 (Waterway Carwash) has frontage along NW Pryor Rd. Lot 13 (multi-tenant building) abuts Lot 11 to the west. The abutting properties to the north are undeveloped pad sites for future commercial development in the Streets of West Pryor. To the south within the same development are a grocery store, multi-tenant commercial buildings and apartments. Across NW Pryor Rd to the east is the Summit Woods shopping center.

Adjacent Land Uses and Zoning

North:	I-470 and undeveloped Streets of West Pryor pad sites / PMIX
South:	Streets of West Pryor residential and commercial development / PMIX
East (across NW Pryor Rd.):	Summit Woods Shopping Center / CP-2 (Planned Community Commercial District)
West:	Streets of West Pryor residential and commercial mixed-use development / PMIX

Site Characteristics

The subject properties are currently undeveloped, but are graded and pad site ready. Access to the site is provided via the internal private drive network serving the Streets of West Pryor development. Lot 11 has frontage along NW Pryor Rd, but no direct access from said street.

Special Considerations

N/A

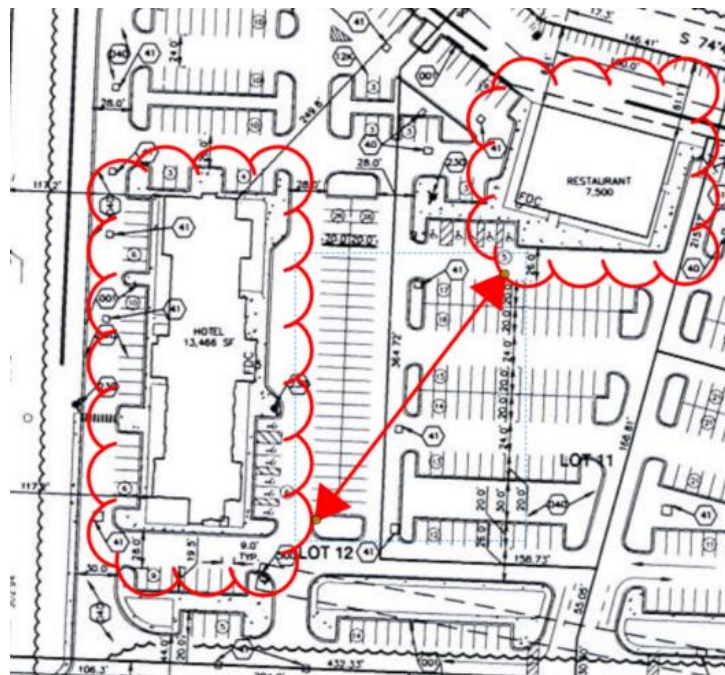
3. Project Proposal

Building Sizes and Uses

The table below provides a side-by-side comparison between the proposed Lot 11 and Lot 13 development and the previously approved plan. Though the use of Lot 11 is proposed to undergo a significant change from a sit-down restaurant to a car wash, the service-oriented nature of the use is compatible with the mixed-use nature of the Streets of West Pryor development as a whole.

	Original Preliminary Development Plan Building Square Footage/Use	Proposed Preliminary Development Plan Building Square Footage/Use
Lot 11	One 7,500 sq. ft. sit-down restaurant	One 9,347 sq. ft. dual tunnel car wash and gas pump island canopy
Lot 13	53,800 sq. ft. (105 rooms) hotel ¹	12,000 sq. ft. multi-tenant building

¹ – The proposed Lot 13 was previously identified as a hotel site. A 7,500 sq. ft. restaurant location was previously identified to the north and east of the hotel site. The developer will swap the hotel and restaurant site locations as depicted below.



Structure(s) Design

Number and Proposed Use of Buildings
1 dual-tunnel carwash and gas pump island canopy (Lot 11) 1 multi-tenant restaurant building (Lot 13)
Building Height
27'-3" – Waterway Carwash (Lot 11) 28' – multi-tenant restaurant (Lot 13)
Number of Stories
1 (Lot 11 and 13)
Parking Provided
14 parking spaces + 35 vacuum stalls / 8 parking spaces required – Lot 11

210 parking spaces provided / 168 parking spaces required – Lot 13

4. Unified Development Ordinance (UDO)

Section	Description
2.040,2.260,2.300,2.320,2.330	Preliminary Development Plans
4.240	Zoning Districts
9.080,9.090,9.150,9.160,9.260	Signs

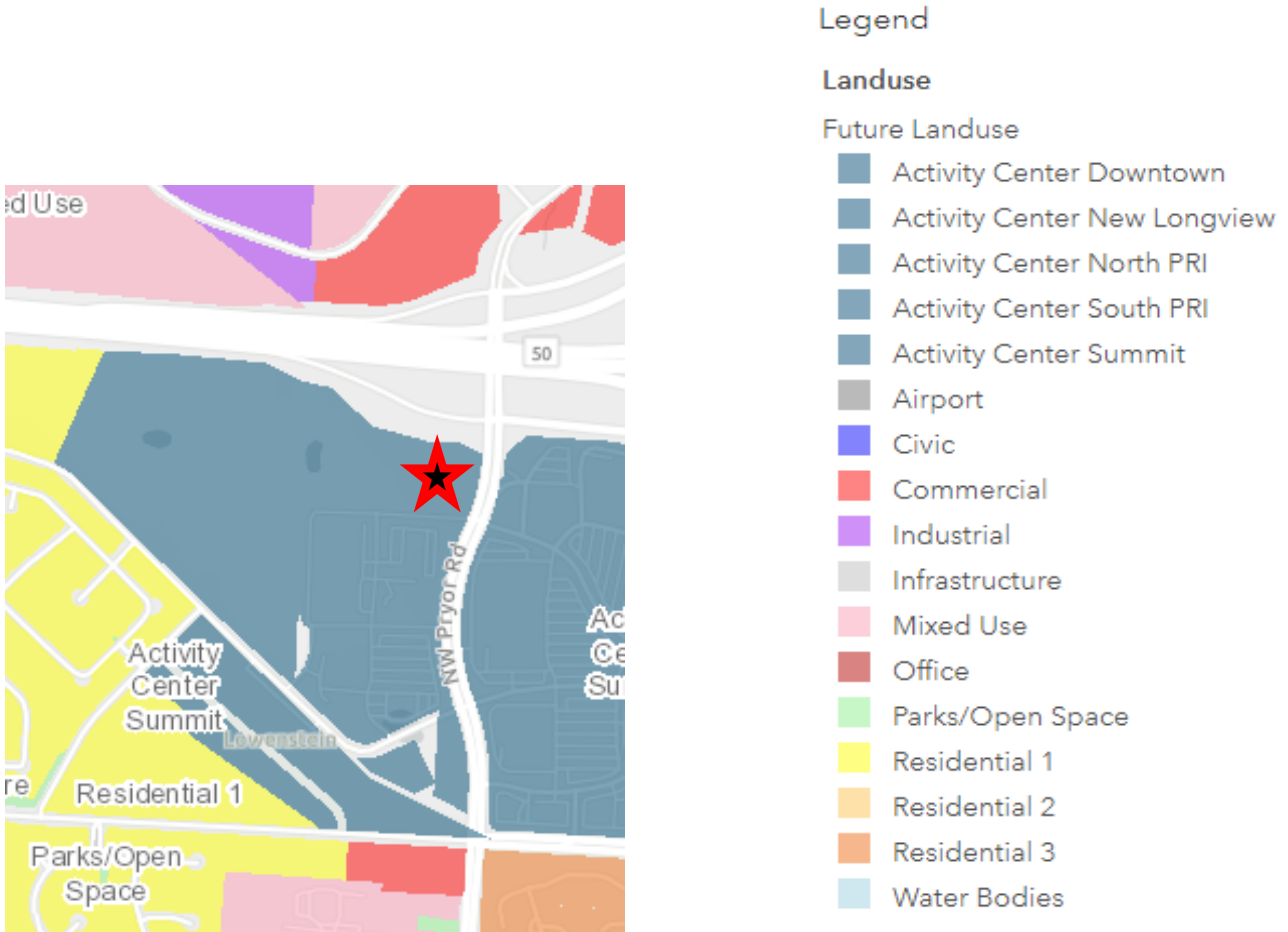
Unified Development Ordinance

Restaurants are an identified allowed use under the existing PMIX zoning district standards approved as part of the 2019 Streets of West Pryor plan. However, car washes were not identified as an allowed use and thus requires a new preliminary development plan for inclusion as an allowed use.

The UDO grants the Planning Commission the authority to consider and approve the applicant’s request to allow signs that exceed the number of signs allowed by right under Section 9.260, Table 9-1.

5. Comprehensive Plan

Focus Areas	Goals, Objectives & Policies
Resilient Economy	Objective: Increase business retention and grow business activity. Objective: Maintain a diverse and valuable tax base.
Summit Activity Center	The Activity Center will continue to serve as a regional retail destination with the potential to serve as a community mobility hub, ensuring increased connectivity to all parts of Lee’s Summit, as well as Interstate 470 and Highway 50.



Comprehensive Plan

The 2020 Ignite! Comprehensive Plan shows the recommended land use for this property as the Summit Activity Center. The Summit Activity Center includes a range of commercial, industrial and residential options. Future development within the Summit Activity Center boundaries will introduce a range of residential densities that are walkable to nearby retail, services and open space opportunities. The Summit Activity Center will continue to serve as a regional retail destination with the potential to serve as a community mobility hub, ensuring increased connectivity to all parts of Lee’s Summit, as well as Interstate 470 and US 50 Highway. The proposed restaurant and car wash uses are compatible with the Summit Activity Center land use designation and vision for providing a range of uses to serve the needs of area residents.

6. Analysis

Background

A preliminary development plan for the subject lots was approved in 2019 as part of the overall 73-acre Streets of West Pryor development. That plan identified and established the uses, square footages, and the number of buildings for each lot within the development. The approved plan identified the proposed Lot 11 as a sit-down restaurant and the proposed Lot 13 as a hotel site. The introduction of a car wash to the approved site of a sit-down restaurant plus a deviation in the proposed buildings’ architecture from that of the approved necessitate new preliminary development plan approval.

- April 6, 2017 – The City Council approved a rezoning (Appl. #PL2016-206) from R-1 and CP-2 to PMIX – Approximately 39 acres generally bounded by I-470 on the north, NW Pryor Road on the east, and NW Lowenstein Drive on the southwest known as West Pryor Village by Ordinance No. 8127.
- January 8, 2019 – The City Council approved a rezoning and preliminary development plan (Appl. #PL2018-098) from R-1 to PMIX for the Streets of West Pryor development by Ordinance No. 8531. Approved uses within the mixed-use development include restaurants, medical office, grocery store, retail and apartments.

Compatibility

Uses. The site is located within the Streets of West Pryor mixed-use development along NW Pryor Rd. and adjacent to the Summit Woods Shopping Center. The proposed buildings and uses are compatible with surrounding commercial and residential land uses, and the overall vision for the Summit Activity Center future land use designation. The service-oriented nature of the proposed use is consistent with the overall character of the previously approved preliminary development plan. However, the introduction of a car wash and the proposed building architecture constitute a substantial change from the previously approved plan, thus requiring approval from the City Council via a new preliminary development plan.

Architecture. The architectural deviation from the previously approved plan is related to the difference in building forms and materials. The proposed building architecture employs a specific look for each of the two building sites. The deviation from the previously approved typical architectural style allows each user to express their own architectural language in a manner that creates its own sense of place and contributes to the architectural interest of the larger development.



Figure 1 - Previously approved typical commercial front elevation.

The proposed car wash building exterior uses architectural metal panel as the primary building material on all sides, wood-look cementitious fiberboard accenting on the northeast building corner and storefront glass. The frequency in use of architectural metal panel systems as a primary building material has increased in newer developments throughout the city for uses that include restaurants, banks, offices, auto dealerships, churches and schools. The use of architectural metal is already present within the Streets of West Pryor development, most notably on Shake Shack, as a primary building material. Staff believes the proposed use of architectural metal panel on the car wash building to be compatible with surrounding development.



Figure 2 - Lot 11 building rendering.

The use of metal panels on the multi-tenant restaurant on Lot 13 is intended to bring visual interest to the building by providing textural and chromatic contrast to differentiate and highlight separate tenant spaces. The proposed building uses a combination of metal panels, EIFS, brick veneer, wood-look cement fiberboard and glass to provide a contemporary aesthetic that is distinct but complementary to other buildings in the development. Staff believes the proposed use of metal paneling on the multi-tenant building to be compatible with surrounding development.



Figure 3 - Proposed Lot 13 front building elevation. Metal panel system shown in copper penny color.

Adverse Impacts

The proposed building architecture and use of metal panels as a primary building material will not seriously injure the appropriate use of, or detrimentally affect, neighboring property. Staff believes the proposed architectural styles to complement and contribute to the range of architectural styles and exterior materials currently found within the Streets of West Pryor mixed-use development.

Public Services

Existing public infrastructure and services are available and adequate to meet the demand of the proposed development.

As part of the proposed preliminary development plan, the applicant provided an updated traffic impact analysis to reflect the proposed land uses and other future uses envisioned within the Streets of West Pryor. The traffic impact analysis was last updated in 2020. The updated analysis shows an increase in trips during both the AM and PM peak hours, but shows a reduction in total daily trips. Most intersections remained at

the same Level of Service (LOS) or improved. Based on this information, no road improvements are recommended as part of this preliminary development plan.

Conditional Materials.

- Required – Metal may only be allowed as a primary exterior wall material with City Council approval.
- Proposed – Aluminum composite metal (ACM) is proposed as the primary exterior building material for the car wash on Lot 11.

Smooth-finished vertical metal panel with reveals are proposed on the east and south elevations of the multi-tenant restaurant building on Lot 13.

- Recommendation – Staff supports the use of the metal panel systems on both proposed buildings as depicted on the building elevations. Staff believes the proposed materials are a positive contribution to the specific architectural aesthetic looking to be achieved for each pad site and to the mix of architecture for the development as a whole.

Sign Package Request.

Number of Signs. The applicant proposes a greater number of signs than allowed by right for both the carwash site and the multi-tenant building.

- **Required –**
 - Three (3) wall signs are allowed by right per building for a single-tenant building.
 - Two (2) wall signs are allowed by right per tenant in a multi-tenant building.
- **Proposed –**
 - **Waterway Carwash (Lot 11):**
 - Four (4) wall signs are proposed for the tunnel carwash building; and
 - Six (6) wall signs are proposed for the gas pump island canopy.

Wall Signs for Waterway (Lot 11)

	Number	Copy	Letter Height	Sign Area/percentage of façade
North Building Elevation ¹	1	“Waterway” + wave logo	5’-4”	122 sq. ft./4.3%
South Building Elevation	1	“Waterway” + wave logo	4’-6”	117 sq. ft./5.1%
South Building Elevation ^{1,2}	1	Waterway” + wave logo	5’	117 sq. ft./51.1%
West Building Elevation	1	“Waterway” + wave logo	3’	53 sq. ft./4.3%

North Gas Pump Canopy Elevation	2	“Waterway Fuels” + “Go Clean”	1’-6” – “Waterway Fuels; 0’-10” – “Go Clean”	29 sq. ft./10.7% - “Waterway Fuels”; 4.35 sq. ft./1.6% - “Go Clean”
South Gas Pump Canopy Elevation	2	“Waterway Fuels” + “Go Clean”	1’-6” – “Waterway Fuels; 0’-10” – “Go Clean”	29 sq. ft./10.7% - “Waterway Fuels”; 4.35 sq. ft./1.6% - “Go Clean”
East Gas Pump Canopy	1	Wave logo	3’-6”	10.6 sq. ft./12.0%
West Gas Pump Canopy	1	Wave logo	3’-6”	10.6 sq. ft./12.0%
PMIX sign standards	3 per single-tenant building; or 2 per tenant in multi-tenant building	N/A	6’ letter height	10% of façade area

¹ – A projecting vertical architectural feature is proposed on the east building elevation. Signs are proposed to be installed on the north- and south-facing sides of said architectural feature. Because the north-facing side of the architectural feature is off-set less than 5’ from the north façade of the building itself, the north-facing side of the architectural feature is considered a continuation of the north building façade for sign area calculation purposes. As a result, the sign on the north side of the architectural feature is calculated to constitute 4.3% of the combined north building and architectural feature façade.

² – Because the south-facing side of the architectural feature referenced above is off-set greater than 5’ (approximately 56’-6”) from the south façade of the building itself, the south-facing side of the architectural feature is considered its own distinct south façade for sign area calculation purposes. As a result, the sign on the south face of the architectural feature is calculated to constitute 51.1% of the south architectural feature façade.

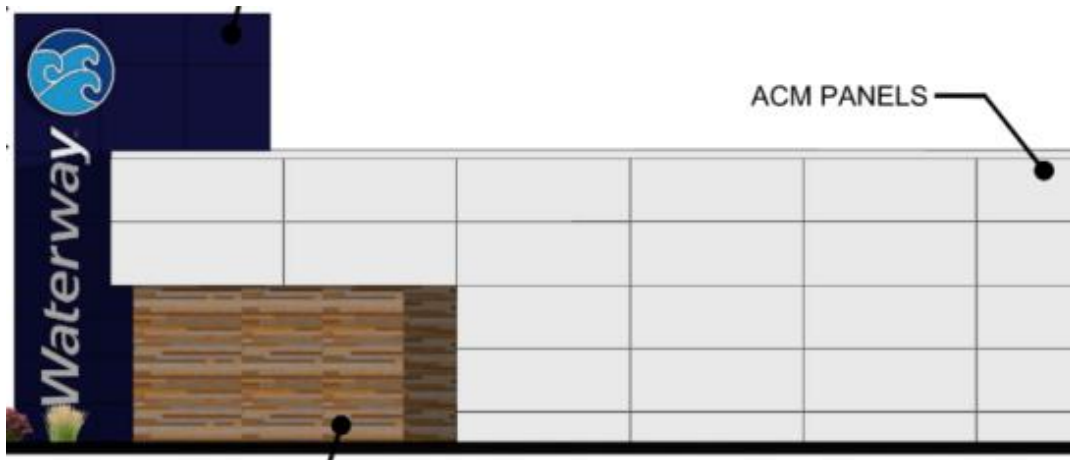


Figure 4 - Partial north building elevation.



Figure 5 - West building elevation.



Figure 6 - Partial south building elevation.

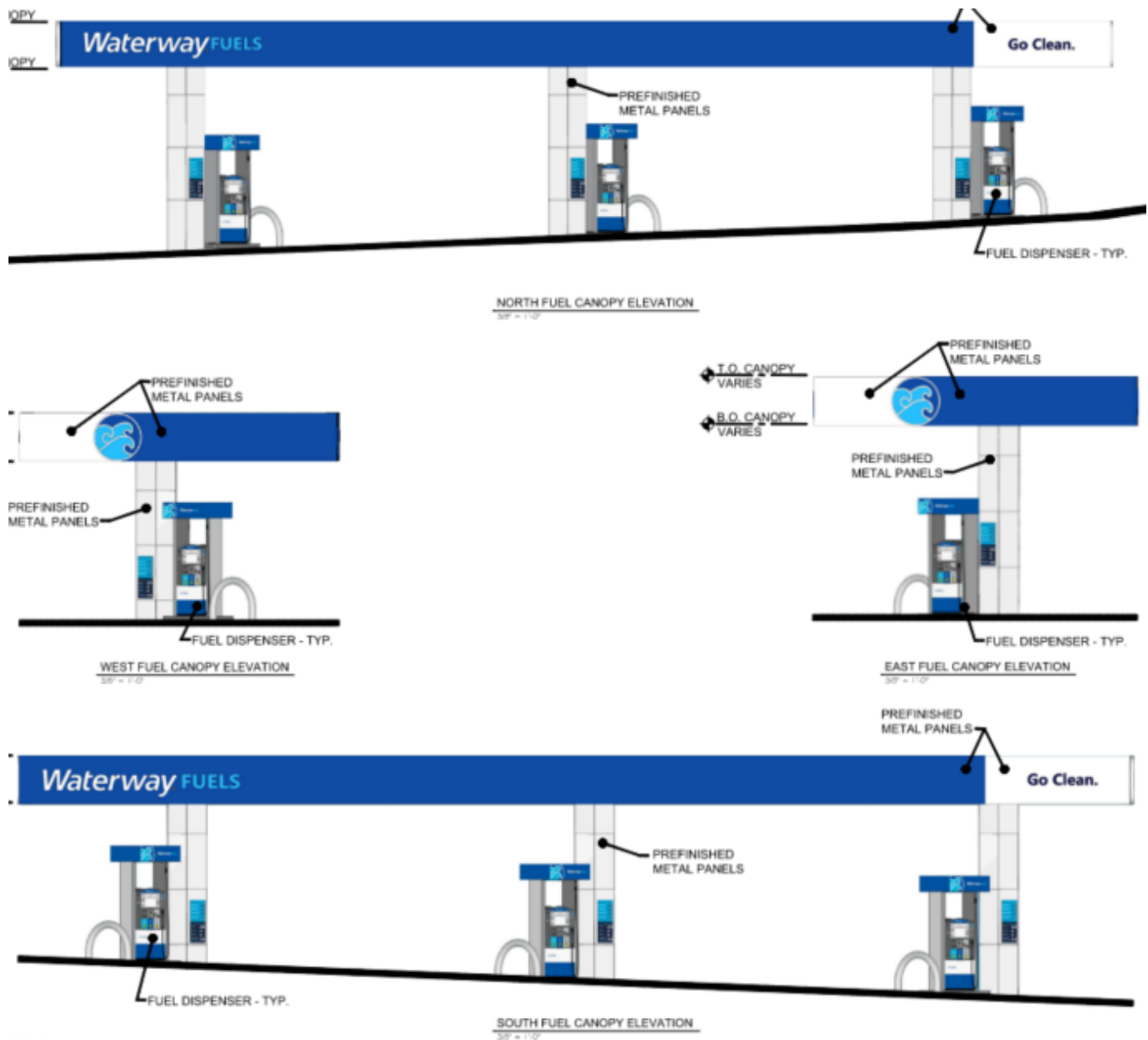


Figure 7 - Pump island canopy elevations.

- **Recommendation** – Staff has historically viewed the allowance of a total of four (4) wall signs on a single-tenant building to be a reasonable request given the number of façade exposures. In an effort to support the goals and intent of the City’s sign ordinance to reduce visual clutter; enhance and protect the physical appearance of the city; and to create a more attractive economic and business climate, staff recommends that the tunnel carwash building and pump island canopy individually be allowed four (4) wall signs. This will require the number of wall signs depicted on the pump island canopy elevation to be reduced from six (6) to four (4).

• **Proposed –**

○ **Multi-tenant Building (Lot 13):**

- A total of three (3) wall signs are proposed for each of the endcap tenant spaces.

Wall Signs for Multi-tenant Building (Lot 13)

	Number	Copy	Letter Height	Sign Area/percentage of facade
North Elevation	1	“30hop”	6’	172 sq. ft./9.1%
South Elevation	1	Tenant Signage placeholder	1’-6”	62 sq. ft./3.1%
East Elevation	3 total (1 per tenant space)	“30hop”; Tenant Signage placeholder; Tenant Signage placeholder	6’ – “30hop”; 1-6” – Tenant Signage placeholders	172 sq. ft./3.1% - “30hop”; 62 sq. ft./7.0%; and 62 sq. ft./6.2%
West Elevation	3 total (1 – “30hop”; 2 – Tenant Signage placeholders)	“30hop”; Tenant Signage placeholder; Tenant Signage placeholder	6’ – “30hop”; 1-6” – Tenant Signage placeholders	172 sq. ft./6.9% - “30hop”; 62 sq. ft./7.9%; and 62 sq. ft./6.8%
UDO standards	2 per tenant in multi-tenant building	N/A	6’ letter height	10% of façade allowed



Figure 8 - East multi-tenant building elevation.

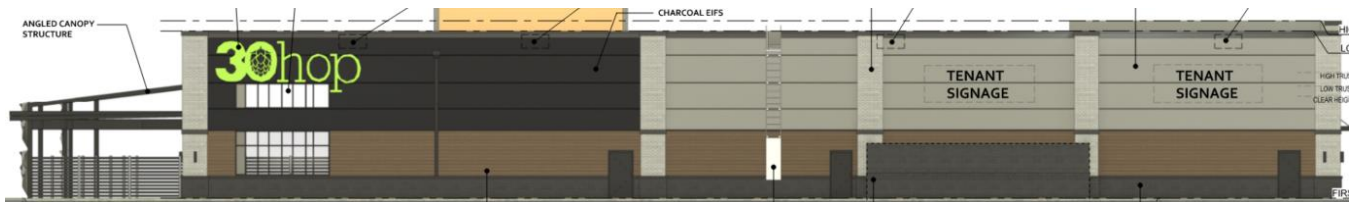


Figure 9 - West building elevation.



Figure 10 - North building elevation



Figure 11 - South building elevation

- **Recommendation** – Staff has historically viewed the allowance of three (3) walls signs for endcap spaces on a multi-tenant building to be a reasonable request given the number of tenant space façade exposures. In an effort to support the goals and intent of the City’s sign ordinance to reduce visual clutter; enhance and protect the physical appearance of the city; and to create a more attractive economic and business climate, staff recommends that each multi-tenant building endcap space on Lot 13 be allowed a total of three (3) wall signs.

Sign Area.

- **Required** – Wall sign size is limited to no more than 10% of the façade area.

• **Proposed –**

- The gas pump island canopy elevations depict signage on all four elevations with sign area from approximately 12% to 12.3%, exceeding the 10% sign area allowance.
- The south face of the vertical architectural feature projecting from the east façade of the tunnel carwash building has signage that constitutes approximately 51.1% of said feature.

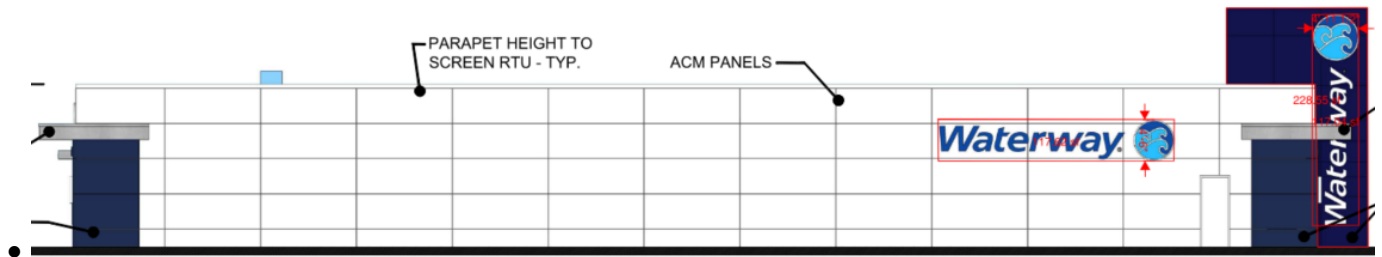


Figure 12 - Proposed south building elevation.

• **Recommended –**

- The gas pump island canopy shall be limited to no more than 10% sign area in accordance with the sign ordinance.
- As previously described, the applicant proposes signage of similar size on both the north- and south-facing sides of the vertical architectural feature projecting from the east building façade. Although the double-faced vertical architectural is of identical size and appearance on both sides, the area of signage allowed on each face is calculated differently due to the difference in how far the faces of the architectural feature are located relative to the north and south building façades. Because the south-facing side of the architectural feature is off-set greater than 5' (approximately 56'-6") from the south façade of the building itself, the south-facing side of the architectural feature is considered its own distinct south façade for sign area calculation purposes. As a result, the sign on the south face of the architectural feature is calculated to constitute 51.1% of the south architectural feature façade. However, staff believes the proposed signage on the south-facing side of the architectural feature be to reasonable because it mirrors signage on the opposite side of the same feature, which is found to be compliant only for the fact that the north façade is less than 5' from the north building façade and therefore is considered an extension of said north façade.

Recommendation

With the conditions of approval below, the application meets the requirements of the UDO and Design and Construction Manual (DCM).

7. Recommended Conditions of Approval

Site Specific

1. Development shall be in accordance with the preliminary development plan signed and sealed July 25,

2023; the Lot 11 building elevations dated July 27, 2023, and August 4, 2023; and the Lot 13 building elevations dated July 21, 2023.

2. A maximum of four (4) wall signs shall be allowed on both the carwash tunnel building and gas pump island canopy on Lot 11. Signage shall comply with all other sign standards (e.g. maximum allowable sign letter height, sign area, etc.) of the PMIX zoning district, except that the wall sign on the south-facing architectural feature located on the east carwash building façade shall be allowed a maximum 51.1% sign area as depicted on the building elevations dated August 17, 2023.
3. A maximum of three (3) wall signs shall be allowed on the two endcap tenant spaces for the multi-tenant building on Lot 13. Signage shall comply with all other sign standards (e.g. maximum allowable sign letter height, sign area, etc.) of the PMIX zoning district.

Standard Conditions of Approval

4. All required engineering plans and studies, including water lines, sanitary sewers, storm drainage, streets and erosion and sediment control shall be submitted along with the final development plan. All public infrastructure must be substantially complete, prior to the issuance of any certificates of occupancy. Please note: Sanitary sewer relocation plans will be required.
5. All Engineering Plan Review and Inspection Fees shall be paid prior to approval of the associated engineering plans and prior to the issuance of any site development permits or the start of construction (excluding land disturbance permit).
6. Certain aspects of the development plan will be further reviewed during the Final Development Plan phase of the project. This includes detailed aspects of the design to help ensure that the plan meets the design criteria and specifications contained in the Design and Construction Manual.
7. Private parking lots shall follow Article 8 of the Unified Development Ordinance for pavement thickness and base requirements.
8. All issues pertaining to life safety and property protection from the hazards of fire, explosion or dangerous conditions in new and existing buildings, structures and premises, and to the safety to fire fighters and emergency responders during emergency operations, shall be in accordance with the 2018 International Fire Code. The use of the building on Lot 13 will determine if additional fire protection is required.
9. The distance to a hydrant on Lot 13 exceeds 300 feet. Provide an additional hydrant.
10. The installation of underground fuel storage tanks (UST's) shall comply with the 2018 International Fire Code and the requirements of the Missouri Department of Natural Resources.
11. If sprinklered, the building on Lot 13 will need to have the FDC within 100 feet of a hydrant.
12. Sign permits shall be obtained prior to installation of any signs through the Development Services Department.